



**ARCHITECTURAL REVIEW BOARD  
MUNICIPAL PLANNING COMMISSION  
-AGENDA-  
Thursday, June 25, 2026 at 7:00 P.M.**

Louis J.R. Goorey Worthington Municipal Building  
The John P. Coleman Council Chamber  
6550 North High Street  
Worthington, Ohio 43085

Watch online at [worthington.org/live](http://worthington.org/live), and comment in person or at [worthington.org/meeting-public-input](http://worthington.org/meeting-public-input)

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**A. Call to Order - 7:00 pm**

1. Roll Call
2. Pledge of Allegiance
3. Approval of minutes of the June 11, 2026 meeting
4. Affirmation/swearing in of witnesses

**B. Architectural Review Board & Municipal Planning Commission – Consent Agenda**

There will be no separate discussion of Consent Agenda items as they are considered to be routine by the Board and Commission and will be adopted by one motion. If a member of the Board & Commission, staff, or public requests discussion on a particular item, that item will be removed from the Consent Agenda and considered separately on the agenda. Approval of Consent Agenda items includes conditions as listed in the staff memo.

**1. Architectural Review Board – Consent Agenda**

- a. Deck Replacement – **697-709 Wesley Ct.** (US Quality Construction) **ARB 33-2026**
- b. Fencing – **123 W. South St.** (Victor Kaczowski) **ARB 35-2026**
- c. Amendments to Previously Approved Building Design – Material Changes – **1012 High St.** (BSD Partners/Mrs. Goodman's) **ARB 36-2026**
- d. Fence – **876 Oxford St.** (Brendan M. Shell-Ayer) **ARB 37-2026**
- e. Signs – **661 High St.** (The Whispering Page) **ARB 38-2026**

**2. Municipal Planning Commission – Consent Agenda**

- a. Conditional Use Permit – Dental Office in C-1 – **1020 High St.** (Kurt A. Lape/The Grove Pediatric Dentistry) **CU 06-2026**

**C. Municipal Planning Commission – Unfinished Business**

1. **Rezoning** – PUD Preliminary Plan – **445 E. Granville Rd.** (Elford Development/  
Boundless) **PUD 01-2026**

**D. Staff Updates**

1. Northeast Area Plan Correction
2. Other

**E. Adjournment**



## MEMORANDUM

TO: Members of the Architectural Review Board  
Members of the Municipal Planning Commission

FROM: Lynda Bitar, Planning Coordinator

DATE: June 18, 2026

SUBJECT: Staff Memo for the Meeting of June 25, 2026

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### **B. Architectural Review Board & Municipal Planning Commission – Consent Agenda**

There will be no separate discussion of Consent Agenda items as they are considered to be routine by the Board and Commission and will be adopted by one motion. If a member of the Board & Commission, staff, or public requests discussion on a particular item, that item will be removed from the Consent Agenda and considered separately on the agenda. Approval of Consent Agenda items includes conditions as listed in the staff memo.

**The details for the non-Elford Development/Boundless Consent Agenda items in this topic were removed from this document.**

**End of deleted Consent Agenda details.**

**C. Municipal Planning Commission – Unfinished Business**

**1. Rezoning – PUD Preliminary Plan – 445 E. Granville Rd. (Elford Development/ Boundless)  
PUD 01-2026**

Previous Hearings:

- January 22, 2026 – Tabled
- May 28, 2026 – On agenda but not discussed

**Findings of Fact & Conclusions**

**Background & Request:**

Zoning: S-1 Zoning District

Original Subdivision: Wright Addition

Business: Boundless

Existing Property Dimensions:

- ~ 660' wide along E. Granville Rd.
- ~ 41 acres

Request: Rezone a 20.4 acre portion of Boundless property to allow residential development.

**Project Details:**

1. Preliminary Plan Submission Requirements (Section 1174.06)
  - a. Legal descriptions (pages 22-24) are included for the two subareas proposed within the 20.3 acre tract of the property to be divided from the 40.934 Boundless parcel.
    - 1) Sub-area #1 ±20 acres – Most of eastern portion of site and all of the Rush Run ravine corridor
    - 2) Sub-area #2 ±0.4 acres
      - 120' wide x 150' deep on north side of Park Overlook Dr. east of existing single-family
      - 2 single-family residential lots planned
    - 3) A Subdivision application will be needed to divide the property.
      - Preliminary Zoning Plat shows the 20.4 acre tract (page 28 of 58)
      - See exhibit with Lots #1-5 for the basic property division for entire Boundless property (page 25 of 58)
      - Ingress/egress easements and utility easements must be included.

- a. Owners, developers, surveyors, engineers and architects have been identified.
  - b. Date, north arrow and site acreage are shown.
  - c. Existing Conditions (page 27 of 58) – Four buildings are planned for removal
  - d. Preliminary Grading Plan (page 31 of 58)
  - e. Site Layout Plan (page 29 of 58)
  - f. Site Utility Plan (page 30 of 58)
  - g. Dwelling Units/Acre:  
246 multi-family + 2 single-family = 248 units ÷ 20.4 acres = 12.2 units/acre
  - h. Proposed Uses – Dwelling Units and Uses Accessory to Dwellings
  - i. Proposed Phasing
    - 1) Sub-area #1 – One Phase
    - 2) Sub-area #2 – Lots will be developed separately
  - j. Development Standards Text (pages 5-14)
2. Allowable Uses (Section 1174.04)
- a. Sub-area #1
    - 1) Multi-family Dwellings
      - Apartments – 2 & 3 story buildings
      - Certain amount to be priced as work-force housing
      - Application is expected for City incentive program
    - 2) Accessory uses & structures
    - 3) Home Occupations
    - 4) Public Uses
    - 5) Essential Services
  - b. Sub-area #2
    - 1) Single-family dwellings
    - 2) Accessory Uses & Structures
    - 3) Home Occupations
    - 4) Public Uses
    - 5) Essential Services
3. Design Regulations (Section 1174.05)
- a. Site Layout:
    - 1) Two sub-areas proposed in PUD
      - Sub-area #1 – ±20 acres – Most of eastern portion of site and all of Rush Run
        - Multi-family development
        - Two-story buildings
          - o 58.9' east of northeast corner of Sub-area #1
          - o ≤ 88' north of single-family northern property lines
          - o ≤ 168' north of house at 560 Park Overlook Dr.
        - Carriage Houses (two-stories)
          - o ≤ 220' north of single-family northern property lines
          - o ≤ 107' east of closest Boundless building
        - Three-story buildings
          - o ≤ 250' north of single-family northern property lines
          - o ≤ 330' north of house at 560 Park Overlook Dr.

- ≤ 108' from east property line
    - Drive
      - ≤ 45' from east property line
      - ≤ 180' north of single-family northern property lines
    - Rush Run No Build/No Disturbance Zone
  - Sub-area #2 – ±0.4 acres
    - 2 single-family residential lots
    - 60' wide lots
    - 9000 sf in area
    - Required front and rear yards – 25'
    - Required side yard – 6'
    - Required sum of side yards – 12'
    - Maximum building height – 2 ½ stories and 30'
- 2) Access to Sub-area #1
- Proposed roundabout
    - North of main Boundless building
    - Connections
      - Drive to/from E. Granville Rd.
      - North of Boundless buildings/parking
      - South of Boundless buildings/parking
      - East to apartment development
  - Drives between buildings and along eastern edge of apartment development
    - Head in parking at buildings
    - Islands with trees throughout parking
  - All roads/drives privately owned/maintained
  - Emergency access only to Indianola Ave. and Park Overlook Dr.
    - Bollards that can be removed by emergency personnel will block the drive at the right-of-way and at the southern end of the apartment parking lot.
    - Drive to head northeast from Park Overlook Dr.
- 3) Access to Sub-area #2
- Each lot allowed 1 curb cut for a driveway from Park Overlook Dr.
- 4) Multi-use Path
- 11" wide
  - Beginning at Park Overlook Dr. across from Indianola Ave.
  - Curves northwest between new single-family lots and 2 story apartments
  - Continues along west side of development between carriage houses and Boundless drive
  - Connects to existing drive and sidewalk leading to E. Granville Rd.
  - Public use allowed
  - Sharrows planned on Proprietors Rd. south extension

b. Buildings:

1) 248 Dwelling Units

- Apartment Buildings
  - Type A

- 2 stories
    - 2 buildings at south end of site (north of 2 proposed single family lots)
  - Types B, C, D, E
    - 3 stories
    - North of 2 story buildings
  - Carriage Houses
    - 5 buildings along west edge of development
    - 2 stories
  - Affordability
    - Rental rate for at least 74 dwelling units will meet 80% Area Median Income for 10 years
  - 2 Single-family houses on Park Overlook Dr.
- 2) Accessory Buildings/Uses
- Clubhouse
  - Amenity area with pool
  - Dumpster enclosure
  - Other structures to maintain/serve the development
- 3) Design Intent
- Gabled roofs
    - 6:12 roof pitch
  - Balconies/Patios
  - Materials
    - Brick
      - Red/brown brick blend
      - Gray grout
    - Siding
      - Architectural grade composite
      - White – Clapboard style
      - Storm (gray) – Dutch Lap
    - Trim
      - Fiber cement
      - White
    - Asphalt fiberglass dimensional roof shingles
      - Landmark
      - Colonial Slate color

c. Landscaping

- 1) Tree replacement plan included showing trees in the parking lot and around the site
- 2) Detailed landscaping plan needed with Final Plan
- 3) L2.1 – South Property Line Planting Plan included
  - Mounding shown
  - New deciduous and evergreen trees and shrubs planned
  - Leave healthy trees
  - Remove existing invasive plants
  - Typical foundation plantings shown

- d. Lighting  
Lighting plan needed with Final Plan
- e. Screening  
Parking facilities and refuse containers must be permanently screened from adjoining residential uses. Section 1149.03 (b) of the Code should be generally followed and screening must be detailed with the Final Plan
- L2.1 – South Property Line Planting Plan included
- f. Tract Coverage
- Site Area – 20.01 acres
  - Disturbed Area 11.22 acres
  - Impervious Area 6.95 acres
- g. Traffic  
A Traffic Impact Study (TIS) was submitted and reviewed by the City Engineer and City’s engineering consultant. The TIS summary is included in the packet.
- h. Parking
- 1) As stated in Development Standards Text, the plan will comply with the requirements in the Code for parking space size and for not less than one parking space per dwelling unit.
    - 325 parking spaces are proposed for 246 dwelling units.
  - 2) Bicycle parking to be provided – To be shown on the Final Plan
- i. Natural Features
- 1) Tree Preservation/Replacement
    - All trees 6” and larger have been identified and the condition evaluated in consultation with a City Arborist. Alignment was slightly modified to save certain trees.
    - Existing trees that can be saved will be protected during construction with fencing
    - Owner will monitor health of trees throughout construction
    - Preserved trees will be fertilized and pruned (if needed) two years after construction
    - Any trees removed or damaged in conflict with the Tree Preservation Plan will be replaced
    - Tree removal - 135 trees:
      - 88 trees in poor, very poor and dead condition
      - 47 trees in fair or good condition
        - o 1172 caliper inches to be removed in fair to good condition
    - Tree Replacement Plan - 275 trees planned
      - 668 caliper inches to be added
      - 54 trees to meet parking lot requirement (325 parking spaces/6)
      - 91 added for screening from existing residential

- Tree Replacement Fee - \$150 per caliper inch not replaced
  - 1172 caliper inches – 668 caliper inches = 504 x \$150 = \$75,600
- 2) Rush Run Preservation
 

The applicant commits to acquisition and preservation of the Rush Run corridor.
- 3) Request
 

The applicant is requesting the tree replacement fee be waived due to the expense involved in acquiring and preserving the Rush Run corridor and constructing the shared-use path with amenities.
  
- j. Public Area Payments
  - 1) Creation of new dwelling units require a cash payment to the City of \$250/dwelling unit for deposit into the special Parks Fund.
  - 2) 246 multi-family units + 2 single-family units = 248 dwelling units x \$250 = \$62,000
  
- k. Public Space Amenities
 

One Public Space Amenity is required for every 5000 sf of gross floor area.

  - 1) The applicant requests the requirement be met with the shared-use path and potential lighting, benches, trash receptacles, dog-waste bag dispensers, etc. to accompany the path.
  - 2) Street trees will be expected for the two single-family lots on Park Overlook Dr.
  
- l. Signage
  - 1) Entry signage at E. Granville Rd. identifying all site uses
    - Size & design to be presented with PUD Final Plan
    - ≥10’ from right of way
  - 2) Development sign at roundabout exit
    - Size & design to be presented with PUD Final Plan
  - 3) Wayfinding Signage
    - Size & design to be presented with PUD Final Plan

**Worthington Land Use Plans:**

1174.03 General Provisions.

(a) Preliminary Plan. The Preliminary Plan shall be submitted to the Municipal Planning Commission to make a recommendation to the City Council, and which, if approved by the City Council, becomes the Zoning for the property and permits preparation of the Final Plan. The Preliminary Plan shall establish uses and development standards for the property as detailed in drawings and Development Standards Text.

1174.05 Development Standards and Development Standards Text.

Section 1174.06 Preliminary Plan Submission Requirements.

Northeast Area Plan

Primary Uses – South of Rush Run Ravine

(No vehicular connector to Indianola Avenue is planned as part of any development on this site.)

- Class-A office

- Residential
- Institutional

Rush Run – Protection expected

**Service & Engineering Department Comments:**

- Elford's traffic impact study demonstrates that the proposed development would have a negligible impact on congestion or the movement of vehicles at the intersection of Proprietors Road and SR 161. New traffic generated by the proposed apartments can be accommodated at the existing traffic signal without modification. Elford's analysis shows that future traffic growth along SR 161 can be accommodated through signal timing optimization. No public improvements are necessary to alleviate the traffic impacts from the proposed development.
- Internal circulation of vehicles and pedestrians is adequately managed through the proposed sidewalks, drives, and roundabout. The installation of a publicly accessible shared use path connecting Park Overlook Drive to the proposed roundabout is consistent with the Bicycle and Pedestrian Master Plan's goals of creating a trail connection between SR 161 and Indianola Avenue.
- A portion of the building south of the pool extends into the waterline easement so will need to be moved out of the easement. Placement can be shown on the Final Plan.

**Division of Planning Comments:**

- Redevelopment of a portion of the Boundless property has been anticipated for some time and was planned as part of the Northeast Area Plan. Residential was an acceptable use for the area south of Rush Run and preservation of Rush Run was expected. This proposal would allow Rush Run to be preserved by the developer/new property owner for its long term benefit by concentrating the residential on the east side, south of Rush Run.
- The sale of this land would provide Boundless with funds to continue the renovation of the campus and support redevelopment of the E. Granville Rd. frontage with additional office/commercial space.
- Once divided, staff recommends this property not be included in the Architectural Review District. The parcel will not have frontage on E. Granville Rd., and the PUD will dictate the development with any modifications regulated as part of Chapter 1174. As the Boundless campus would not be part of the rezoning, inclusion of the new parcel south of Rush Run housing the Boundless campus should be retained in the Architectural Review District.
- Emergency-only access to the site on the south side, at Park Overlook Dr. and Indianola Ave., has long been a desire of the Colonial Hills residents and the City. This rezoning would formalize that access point for the long term.
- Currently, the Boundless site hosts many residents with and without dogs that traverse through and around the site. The planned path would allow that activity to continue and adds amenities for active transportation on the east side of the site. Boundless is also planning creation of a path around the campus and will seek approval at a future ARB meeting.

- Site layout
  - Proposed layout of the access and parking drives should provide for ease of movement and convenient access to parking around the entire site.
  - Location of the 2-story buildings closer to the single-family residential should help mitigate any effect of the 3-story buildings.
  - Parking is not planned directly adjacent to single-family residential.
  - The addition of two single-family lots on Park Overlook Dr. should also help reduce the impact of the development.
  - L2.1 – South Property Line Planting Plan would add additional screening of the site from Park Overlook Dr.
- Building design was modified to better complement Colonial Hills and Worthington generally, including the use of red/brown brick and more design detail.
- The addition of some housing that is affordable to people making 80% of the Area Median Income is desirable.
- The amount of parking exceeds the amount required by the PUD Code by 50%.
- The Final Plan and Subdivision Plat must illustrate a no disturbance area and convey the following information: *There shall be no disturbance in the area identified on the Final Plan and Final Plat as “No Disturbance” without express written approval of the City. This includes removal of any trees and vegetation that are not considered invasive or noxious by the City.*

**Recommendation:**

**Approval** of the following motion is recommended based on the comments above:

**THAT THE REQUEST BY UNDERHILL & HODGE, LLC ON BEHALF OF ELFORD DEVELOPMENT FOR APPROVAL OF A PUD PRELIMINARY PLAN TO CONSTRUCT HOUSING ON A 20.4 ACRE PORTION OF THE PROPERTY AT 445 E. GRANVILLE RD. AS PER CASE NO. PUD 01-2026, DRAWINGS NO. PUD 01-2026, DATED JUNE 17, 2026, BE RECOMMENDED TO THE CITY COUNCIL FOR APPROVAL BASED ON THE PLANNING GOALS OF THE CITY, AS REFERENCED IN THE LAND USE PLANS, AND ON THE FINDINGS OF FACT AND CONCLUSIONS IN THE STAFF MEMO AND PRESENTED AT THE MEETINGS.**

**D. Staff Updates**

1. Northeast Area Plan Correction

There are three references to residential as a Primary Use in the Boundless Mixed-Use Zone:

- Pages 12 & 50 (online [https://worthington.org/DocumentCenter/View/11501/Worthington-Northeast-Area-Plan\\_FINAL\\_ADOPTED-JULY-2024-Reducedpdf-?bidId=](https://worthington.org/DocumentCenter/View/11501/Worthington-Northeast-Area-Plan_FINAL_ADOPTED-JULY-2024-Reducedpdf-?bidId=)) and below (see next pages):
  - "Low density residential" is a Primary Use for the Andover frontage
  - "Residential" (with no additional descriptor) is a Primary Use for the area south of Rush Run Ravine



**Eco Industrial Zone**

**Primary Uses**

- Light manufacturing, advanced industries, renewable and clean energy industries
- Tech-flex spaces (high bay, divisible)
- Class-A office space
- Research & Development
- Green infrastructure (for renewable energy, electric vehicles, etc.)
- Private open space and plazas

**Mixed Use Zone**

**Primary Uses**

- Light manufacturing, advanced industries, renewable and clean energy industries
- Tech-flex spaces (high bay, divisible)
- Class-A office
- Research & Development
- Green infrastructure (for renewable energy, electric vehicles, etc.)
- Private open space and plazas

**Secondary Uses**

- Residential
- Retail & services
- Food & beverage
- Amenities in support of live-work environment

**Boundless Mixed-Use Zone**

**Primary Uses – SR-161 Frontage**

- Class-A office
- Amenities in support of live-work environment
- Private open space and plazas

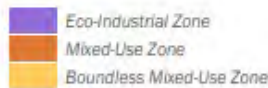
**Primary Uses – Andover Frontage**

- Low density residential

**Primary Uses – South of Rush Run Ravine**

(No vehicular connector to Indianola Avenue is planned as part of any development on this site.)

- Class-A office
- Residential
- Institutional



◀ Eco Innovation District Land Use Recommendations Plan



### Eco Industrial Zone

#### Primary Uses

- Light manufacturing, advanced industries, renewable and clean energy industries
- Tech-flex spaces (high bay, divisible)
- Class-A office space
- Research & Development
- Green infrastructure (for renewable energy, electric vehicles, etc.)
- Private open space and plazas

### Mixed Use Zone

#### Primary Uses

- Light manufacturing, advanced industries, renewable and clean energy industries
- Tech-flex spaces (high bay, divisible)
- Class-A office
- Research & Development
- Green infrastructure (for renewable energy, electric vehicles, etc.)
- Private open space and plazas

#### Secondary Uses

- Residential
- Retail & services
- Food & beverage
- Amenities in support of live-work environment

### Boundless Mixed-Use Zone

#### Primary Uses – SR-161 Frontage

- Class-A office
- Amenities in support of live-work environment
- Private open space and plazas

#### Primary Uses – Andover Frontage

- Low density residential

#### Primary Uses – South of Rush Run Ravine

(No vehicular connector to Indianola Avenue is planned as part of any development on this site.)

- Class-A office
- Residential
- Institutional

◀ Eco Innovation District Land Use Recommendations Plan

- Page 63 (online [https://worthington.org/DocumentCenter/View/11501/Worthington-Northeast-Area-Plan\\_FINAL\\_ADOPTED-JULY-2024-Reducedpdf-?bidId=](https://worthington.org/DocumentCenter/View/11501/Worthington-Northeast-Area-Plan_FINAL_ADOPTED-JULY-2024-Reducedpdf-?bidId=)) and below:
  - "...residential (low density)..." is a recommendation for the Boundless Mixed-Use Area but the two sub-areas – “Andover Frontage” and “South of Rush Run Ravine” – are not delineated.

Northeast Area Plan

**Route 161 Gateway Hub:**

The Route 161 Gateway Hub is at the intersection of Huntley Road and Route 161. It is currently occupied by commercial uses including a well-established car dealership. With Route 161 being an important east-west roadway connecting across the region and having a direct interchange with Interstate 71, this area is clearly the primary gateway into the Eco Innovation District. As such, the Plan recommends that future redevelopment of this hub focus on a mix of uses that supports the businesses and employees, provides an attractive and welcoming environment, and promotes the brand and identity of the Eco Innovation District. The Route 161 frontage should be more regionally focused with office and commercial uses and the Huntley Road frontage should provide ground-level activation with retail, food and beverage, business services and workforce housing above.

**Proprietors Road Mixed-Use Corridor:**

The Proprietors Road Mixed-Use Corridor is unique onto itself and serves as a transition between the Northeast industrial area and railway on the east, to the residential single-family neighborhoods to the west. The corridor has traditionally been a blend of mixed residential on the west side of the street and commercial enterprises on the east. The Plan recommends that this area remain a mix of uses, including vertical mixed-use with workforce housing located over retail or commercial space at the ground level. The Ohio Railway Museum is a unique asset in this corridor and should be strengthened and improved as a local attraction. The Plan also recommends that new development be encouraged to front directly on Proprietors Road with active ground level uses, and parking located behind the building frontages. This is especially recommended along the railway, with parking and enhanced landscape used to provide a visual and sound buffer to the rail line.

**Boundless Mixed-Use Area:**

The Boundless Mixed-Use Area includes a 45-acre campus owned by Boundless Health, a not-for-profit health and wellness organization. Boundless Health plans to sell about 25 acres of their campus in this unique area of Worthington, along Route 161 and with Rush Run crossing it. This area abuts existing single-family neighborhoods to the south and west. As such, the Plan recommends that future land uses in this area focus on Class-A office space (likely along Route 161), residential (low density) and institutional (likely education oriented) in the same mixed-use live-work environment envisioned for the rest of the district. Rush Run should be preserved and enhanced as an open space amenity and natural riparian corridor as it bisects this area.

The primary access point to the Boundless campus aligns with the Route 161/Proprietors Road intersection. This access point will continue to serve this area and no other road connection is recommended. No vehicular connection to Andover Street or Indianola Avenue is recommended. The shared use path along Proprietor's Road can connect into Indianola Avenue, south of SR 161.

Per the discussions at the Northeast Area Plan Steering Committee meetings and the presentations to the Municipal Planning Commission and City Council, “Residential”, without a designated number of units, was proposed for the Boundless Mixed-Use Area South of Rush Run Ravine. Either of the following changes could be made to the sentence below from Page 63 of the Northeast Area Plan to take care of the discrepancy:

1. “As such, the Plan recommends that future land uses in this area focus on Class-A office space (likely along Route 161), residential (~~low density~~) and institutional (likely education oriented) in the same mixed-use live-work environment envisioned for the rest of the district.”
2. “As such, the Plan recommends that future land uses in this area focus on Class-A office space (likely along Route 161), residential (low density ~~on Andover frontage~~) and institutional (likely education oriented) in the same mixed-use live-work environment envisioned for the rest of the district.”

**Recommendation:**

Staff requests one of these changes be recommended to the City Council for adoption.

**Motion:**

**THAT PAGE 63 OF THE FINAL ADOPTED NORTHEAST AREA PLAN BE MODIFIED AS SHOWN IN CHANGE #\_\_.**



**MPC APPLICATION  
PUD 01-2026  
455 E. Granville Rd.**

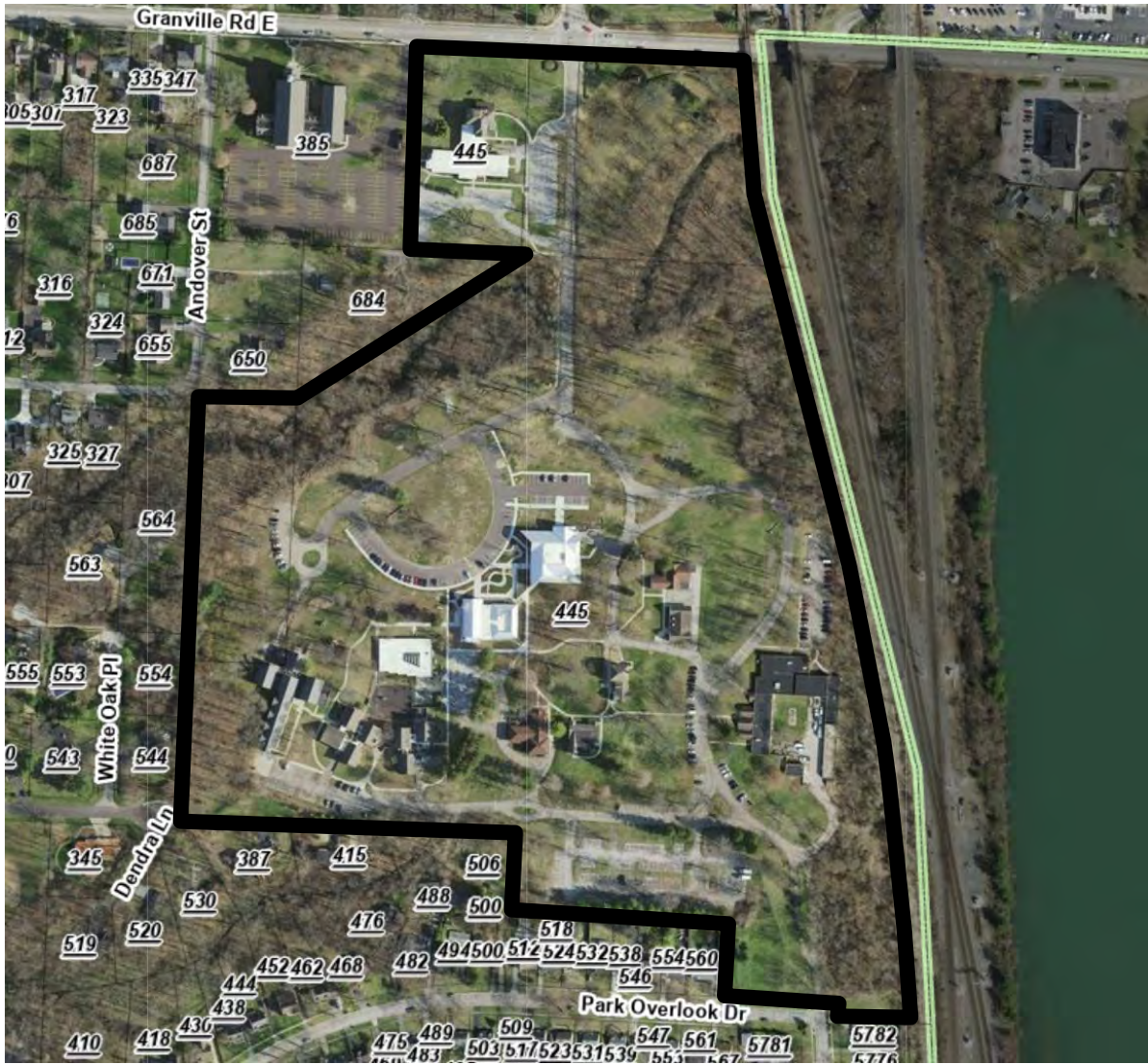
<b>Plan Type:</b> Planned Unit Development	<b>Project:</b>	<b>App Date:</b> 01/09/2026
<b>Work Class:</b> Preliminary Plan	<b>District:</b> City of Worthington	05/22/2026
<b>Status:</b> In Review		06/17/2026
<b>Valuation:</b> \$0.00		<b>Hearings:</b> MPC 01/22/2026 - Tabled
<b>Description:</b> Rezone 20.4 acres at southeast corner of Boundless site as a PUD to allow multifamily development		MPC 05/28/2026 - On agenda but not discussed

<b>Parcel:</b> 100-000696	Main	<b>Address:</b> 445 E Dublin-Granville Rd Bldg A Worthington, OH 43085	Main	<b>Zone:</b> Planned Unit Development
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<b>Owner</b> Boundless Health Inc. 445 E DUBLIN-GRANVILLE RD BLDG G Worthington, OH 43085	<b>Developer</b> ELFORD, INC. 1220 DUBLIN ROAD COLUMBUS, OH 43215	<b>Applicant</b> David Hodge 8000 Walton Pkwy. Ste. 260 New Albany, OH 43054 Home: (614) 306-4649 Business: (614) 306-4649 Mobile: (614) 306-4649	<b>Applicant</b> Eric Zartman 8000 Walton Parkway, Suite 260 New Albany, OH 43054 Business: (614) 335-9328
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Invoice No.	Fee	Fee Amount	Amount Paid
INV-00008461	Preliminary Development Plan	\$850.00	\$850.00
<b>Total for Invoice INV-00008461</b>		<b>\$850.00</b>	<b>\$850.00</b>
<b>Grand Total for Plan</b>		<b>\$850.00</b>	<b>\$850.00</b>

# 445 E. Granville Rd.



**DATE //** May 15, 2026  
**ATTN //** Worthington ARB | MPC  
**RE //** Boundless Development

Following our initial presentation to the Worthington ARB | MPC on January 22, 2026, the development team has continued to refine + make design decisions based on the comments + direction put forth by the board members + public input. We feel confident that these design decisions not only respond to what we heard in a thoughtful + detailed manner, but have resulted in a much better product, that all stakeholders, Boundless, Elford, + the City of Worthington can be proud of.

At a high-level, we heard from both board members + the public that our initial proposal lacked the architectural character + detail that Worthington is known for; that this should be of Worthington, for Worthington. Outlined below are the design changes that reflect the sentiment above that are presented in this submission:

- **Change in Architectural Styling + Direction.**
  - The proposed design moved away from the previous 'Nantucket' architectural style towards the American Federal style, which is consistent in the core of Old Worthington.
- **Change in Materiality, Variation, + Detailing.**
  - Critical remarks of the previous design iteration included: lack of variety across the site + building types (ie. sameness), too neutral of a material palette with minimal to no distinction, too simplistic of detailing, lack of overall character, etc.
  - In response, the proposed design replaced the stone single-story base consistent across all footprints with a tumbled red-brown brick blend. The use of red-brown brick varies across building footprints + within building footprints, from one-story to two-stories, creating a much more diverse streetscape as you travel within the development.
  - There are now three distinct color + material palettes, white siding, gray siding, + red-brown brick. Each color palette follows a strict order of details + styling, which creates a varied, yet ordered system throughout the development.
  - The proposed materials, style, + associated details reflect + call attention to similar detailing + styling found in Old Worthington.
- **Change in Roof Slopes to Reduce Overall Heights.**
  - Originally proposed with a 12:12 roof slope, we received feedback that the footprints read too tall + did not respond to the much smaller, lower-sloped roofs of our neighbors. As a response, we are now proposing a 6:12 roof slope, which brings the perceived height of the proposed buildings down significantly.
  - This roof change also better aligns with the new architectural styling + detailing.
  - In addition to the lower roofs, we have also adjusted the massing of each building footprint, reducing the number of gable ends, resulting in smaller, more disciplined building elevations.



- **Closed 'Breezeway' Conditions at Southernmost Building Footprints.**
  - As previously proposed, we are maintaining the two-story buildings along the southern edge of the proposed development. This design move was received well during our first meeting, as we bring the scale of our proposed buildings down to fit better contextually.
  - In addition, we have added 'wrap-around' units along the southern edge of these two building footprints, which terminate the 'breezeway' condition. In doing so, along with the updated detailing + massing updates listed above, these buildings read much more residential in nature, and less of a multi-family 'breezeway' building.
- **Site Plan Adjustments + Building Footprint Locations.**
  - With the new architectural language developed above, we made several adjustments to the overall site plan, specifically in regards to the locations of the different building types.
  - The proposed development includes six unique building types, which is more complex than your traditional multi-family development found here in Central Ohio. Each building type is a unique composition of the three color + material palettes (listed above), which we have re-shuffled building type locations so that neighboring building types are different. As a result, this leads to a much more varied + dynamic experience within the development.
  - Also, we have shifted the two-story building footprint that parallels the southern property edge further east. This was done in an effort to minimize the visual impact of the proposed buildings directly behind the adjacent neighboring residences.
- **Site Landscaping + Buffering.**
  - In addition to the shifting of building footprints along the southern property line, the development team has extensively studied the existing conditions + have designed a layered approach to providing additional buffering to this side of the site.
  - Additional tree plantings, along with mounding, are proposed to reduce sightlines + visual impact of the proposed development.

In summary, we believe these solutions enhance the overall design of the proposed development, while complementing + blending in with the charm + character that defines Old Worthington.

As always, we are excited to continue this discussion with City Staff and the Board, as we are firm believers that through a collaborative design process with passionate, mission-driven stakeholders, the best solutions will arise and be implemented to positively impact the greater good.

Jonathan Grubb  
Director of Design, Architect.



CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026



**PLANNED UNIT DEVELOPMENT TEXT**  
**May 20, 2026**

**I. INTRODUCTION**

This Planned Unit Development application is submitted by Elford Development, the Applicant and Developer of record. Elford is an Ohio-based development and construction firm with extensive experience delivering high-quality residential and mixed-use developments, with an emphasis on thoughtful site planning, architectural quality, and long-term compatibility with surrounding communities. Elford is acting with the authorization and cooperation of the property owner, Boundless Strategic Resources Inc., a nonprofit organization providing behavioral health, developmental disability, and supportive services to individuals and families throughout Central Ohio.

The subject property is located at 445 East Granville Road in the City of Worthington and consists of approximately 40.934 acres developed historically as a low-intensity institutional campus with direct access to East Granville Road. The property is currently zoned S-1 (Special District). The surrounding context includes established R-10 single-family residential neighborhoods to the south and west, with institutional and corridor-oriented uses located east and north along East Granville Road. A railroad line runs along the east perimeter of the site. Due to its size, configuration, and historic campus-style use, the property is distinct from surrounding residential parcels and is well-suited for a coordinated, planned redevelopment approach.

As part of this application, the eastern portion of the Boundless campus, consisting of  $\pm 20.4$  acres, consisting of Subarea 1 ( $\pm 20.0$  acres) and Subarea 2 ( $\pm 0.4$  acres) will be subdivided and conveyed to the Applicant for residential development. The remaining portion of the property will remain under Boundless ownership and continue its institutional use. This PUD application applies exclusively to the newly created eastern parcel and is structured to ensure that the proposed development does not interfere with or diminish the continued operations of Boundless on the retained property.

The proposed subdivision and rezoning are consistent with the City's adopted planning framework. The Worthington Northeast Area Plan identifies the Boundless campus as an appropriate location for thoughtful evolution and mixed-use redevelopment, including residential uses on select portions of the site, while emphasizing neighborhood compatibility, protection of natural features, and controlled access. The Worthington Comprehensive Plan recognizes large institutional properties along the East Granville Road corridor as key redevelopment opportunities and encourages intentional reuse supported by strong design and environmental stewardship. Specifically, for this OSU Harding Hospital Site, the proposed development directly implements the Comprehensive Plan's vision for a campus-style redevelopment that preserves wooded ravines, provides natural buffers and public path connections, and introduces new residential development compatible with surrounding neighborhoods and the East Granville Road corridor. In addition, the City's Housing Needs Assessment documents ongoing demand for new housing options and identifies redevelopment of large, underutilized sites as a necessary strategy in a largely built-out community. The proposed PUD advances these policies by introducing new housing in a planned and context-sensitive manner without encroaching on established neighborhoods.



The Applicant proposes to rezone the subdivided eastern parcel from S-1 to a Planned Unit Development (PUD) pursuant to Chapter 1174 of the Codified Ordinances of the City of Worthington. The PUD is organized into two subareas:

- Subarea 1 is  $\pm 20.0$  acres in area. It consists of the primary multifamily residential development and is governed by project-specific development standards. This subarea includes apartment buildings, carriage houses, garage, clubhouse, and pool, with a total of 246 dwelling units proposed. This subarea also provides  $\pm 6.25$  acres of open space reserve.
- Subarea 2 is  $\pm 4$  acres in area. It consists of two single-family lots (Lots 1 and 2) located along the southern edge of the PUD area. These lots are subject to development standards which complement the neighborhood's existing R-10 zoning district and are intended to function as a buffer and transition between the multifamily development and the adjacent single-family neighborhood.

The proposed development is informed by early coordination with City staff and community stakeholders and incorporates several guiding principles reflected throughout this PUD. These include preservation of significant natural features, including a designated reserve area along the northern portion of the site; careful management of access to protect adjacent residential streets; and the incorporation of a publicly accessible shared-use path network, to be made available through an easement granted to the City of Worthington. Together, these elements reinforce the project's emphasis on environmental stewardship, neighborhood compatibility, and connectivity.

Through subdivision and rezoning to a Planned Unit Development, the proposed project replaces S-1 zoning with a modern, predictable regulatory framework that implements adopted plans, provides needed housing, preserves natural features, and ensures long-term compatibility with surrounding development.

## **II. PERMITTED USES**

The following uses are permitted within the Planned Unit Development, subject to the applicable development standards set forth in this Development Standards Text and as depicted on the approved Preliminary Plan.

### **A. Subarea 1 – Multifamily Residential**

Subarea 1 is intended to accommodate the primary residential development within the PUD. Permitted uses within Subarea 1 include:

1. Multifamily residential dwellings, including apartment buildings.
2. Carriage house dwelling units and similar small-scale residential buildings containing dwelling units.

3. Accessory uses customarily incidental to multifamily residential development, including but not limited to:
  - a. Leasing and on-site management offices;
  - b. Resident amenities and common areas;
  - c. Indoor and outdoor recreational facilities;
  - d. Parking areas and garages; and
  - e. Maintenance and service facilities serving the development.
4. Open space, landscaping, and natural areas, including pedestrian pathways, shared-use paths, and stormwater management facilities.
5. Utilities and infrastructure necessary to serve permitted uses, Public Uses, Essential Services

All uses within Subarea 1 shall be developed in accordance with the site-specific standards established in this Planned Unit Development.

**B. Subarea 2 – Single-Family Transition Lots**

Subarea 2 consists of two single-family residential lots intended to provide a transition between the multifamily development and adjacent single-family neighborhoods. Permitted uses within Subarea 2 shall include the following:

1. Single-family detached dwellings.
2. Public uses.
3. Essential services.
4. Accessory uses and structures customarily incidental to a permitted principal use, including garages and residential accessory buildings.
5. Home occupations, conducted in accordance with the applicable provisions of the Planning and Zoning Code.

**III. DEVELOPMENT STANDARDS**

**Subarea 1 – Multifamily Residential**

The following development standards apply to Subarea 1 of the Planned Unit Development. All development within Subarea 1 shall comply with these standards and the approved Preliminary Plan and associated exhibits.

A. Design Regulations (Worthington Planning and Zoning Code Section 1174.05(a))

1. Character

- a. Subarea 1 shall be developed as a cohesively planned multifamily residential community.
- b. Development shall consist of a coordinated arrangement of apartment buildings, carriage house buildings, garage, club house, pool, internal private streets, pedestrian pathways, parking areas, and open space.
- c. The overall development pattern shall reflect a campus-style layout, consistent with the approved Preliminary Plan.
- d. The development shall provide an appropriate transition in scale and intensity between multifamily residential buildings, open space areas, and adjacent site boundaries.

2. Design

a. Design Intent

- i. Building design, architectural character, and exterior materials shall be generally consistent with the submitted Elevations and Materials exhibit, as approved with the Preliminary Plan.

b. Permitted Building Types

- i. Apartment buildings.
- ii. Carriage house residential buildings.
- iii. Accessory buildings customarily incidental to multifamily residential use.

c. Maximum Building Height

- i. Apartment buildings shall not exceed three (3) stories in height.
- ii. Carriage house buildings shall not exceed two (2) stories in height.

d. Number of Dwelling Units

- i. A maximum of 246 dwelling units shall be permitted within Subarea 1.

e. Building Location and Arrangement

- i. Buildings shall be located within the development areas depicted on the approved Preliminary Plan.
    - ii. Building placement and orientation shall be generally consistent with the approved Site Plan and architectural exhibits.
  - f. Exterior Building Materials
    - i. Permitted exterior building materials include:
      - (a). brick or masonry;
      - (b). composite siding or panels;
      - (c). architectural-grade composite siding; and
      - (d). other comparable materials approved with the Final Plan.
    - ii. Exterior materials shall be durable and appropriate for long-term residential use.
- 3. Screening
  - a. Parking areas, service areas, and utility equipment shall be screened where appropriate in accordance with the screening, fencing, and landscaping provisions of Chapter 1180 (Fences, Shrubbery, and Hedges) of the Worthington Planning and Zoning Code.
  - b. Screening may consist of landscaping, fencing, walls, or a combination thereof.
  - c. Screening treatments shall be shown on the Final Plan.
- 4. Tract Coverage
  - a. Buildings, internal streets, parking areas, and open space shall be arranged in general conformance with the approved Preliminary Plan.
  - b. Subarea 1 shall include landscaped open space and common areas distributed throughout the site.
  - c. Open space areas may include lawn areas, pedestrian plazas, seating areas, and stormwater facilities designed as site amenities.
- B. Traffic and Parking (Worthington Planning and Zoning Code Section 1174.05(b))
  - 1. Vehicular Access
    - a. Primary vehicular access to Subarea 1 shall be provided from East Granville Road, as depicted on the approved Preliminary Plan.

- b. Internal private streets shall provide access to buildings, parking areas, and service areas within Subarea 1.
- 2. Emergency Access
  - a. A secondary access point located at the southern edge of Subarea 1, as shown on the approved Preliminary Plan, shall be limited to emergency access only.
  - b. This access shall be controlled by removable or knockdown bollards.
  - c. Construction access at this location is prohibited.
  - d. Details of emergency access control, including bollard type and placement, shall be addressed with the Final Plan.
- 3. Internal Circulation
  - a. Internal streets and drive aisles shall be designed to accommodate residential traffic, service vehicles, and emergency vehicles.
  - b. Internal circulation shall be generally consistent with the approved Preliminary Plan.
- 4. Parking
  - a. Off-street parking shall be provided in accordance with the approved Preliminary Plan and shall comply with Section 1174.05(b)(2)(C) of the Worthington Planning and Zoning Code.
  - b. Parking may include surface parking areas and garage parking associated with carriage house buildings.
  - c. Accessible parking spaces shall be provided in accordance with applicable law.
  - d. Bicycle Parking shall comply with Section 1174.05(b)(2)(D) of the Worthington Planning and Zoning Code.
- C. General Requirements (Worthington Planning and Zoning Code Section 1174.05(c))
  - 1. Utilities
    - a. Utilities serving Subarea 1 shall be placed underground where practicable.
    - b. Above-ground utility equipment shall be screened where feasible.

2. Natural Features

- a. Natural features within Subarea 1, including wooded areas and the Rush Run corridor, shall be preserved to the greatest extent practicable. Extent and protection language shall be shown on the Final Plan.
- b. Tree preservation, removal, and replacement shall be conducted in accordance with the submitted Tree Preservation Plan, which identifies trees to be preserved, trees approved for removal, and replacement or mitigation measures.
- c. The Tree Preservation Plan prioritizes preservation of mature trees and woodland areas while accommodating necessary site development.
- d. The Tree Preservation Plan may be modified and approved as part of the Final Development Plan.
- e. The boundaries and general extent of the Reserve Area are shown on the approved Preliminary Plan.

3. Environmental Protection

- a. Stormwater management facilities shall be designed and constructed in accordance with approved engineering plans.
- b. Stormwater facilities may be integrated into landscaped open space areas.

4. Lighting

- a. Site lighting shall be provided to ensure safety and security within Subarea 1 for residential use.
- b. Lighting shall be designed to minimize glare and light spill onto adjacent properties.
- c. Lighting fixtures, mounting heights, and illumination areas shall be shown on the Final Plan.

5. Signage

- a. Entry Signage
  - i. Entry signage shall be permitted to provide primary identification for the overall site.
  - ii. Location shall be at least 10' from the E. Granville Rd. right of way.
  - iii. Size and design shall be presented with the Final Plan
- b. Development Monument Signage

- i. One development monument sign shall be permitted at the roundabout exit serving the primary residential entrance.
    - ii. The purpose of the monument sign is to mark the main entrance to the residential development.
    - iii. Size and design shall be determined as part of the Final Plan in coordination with City staff.
  - c. Wayfinding Signage
    - i. Up to 5 wayfinding signs shall be permitted internal to the residential development.
    - ii. The purpose of wayfinding signage is directional guidance for residents and visitors.
    - iii. Size and design shall be determined as part of the Final Plan in coordination with City staff.
- 6. Phasing
  - a. Development of Subarea 1 shall occur in one (1) phase, unless otherwise approved by the City.
- 7. Shared-Use Path
  - a. A shared-use path shall be provided within Subarea 1 generally as depicted on the approved Preliminary Plan.
  - b. The shared-use path shall be designed to accommodate pedestrian and bicycle circulation and shall function as part of the internal non-motorized circulation system serving the development.
  - c. The shared-use path shall provide connectivity between residential buildings, open space areas, and other site amenities shown on the approved plans.
  - d. Final alignment, design details, and construction specifications for the shared-use path shall be reviewed and approved as part of the Final Plan.
- 8. Residential Affordability
  - a. A minimum of 74 dwelling units shall be allocated to work force housing with rental rates at 80% of Area Median Income for a period of 10 years
- D. Divergence
  - 1. 1174.05(c)(2)(B) – Natural Features. The Applicant requests a divergence from, and a full waiver of, the natural feature replacement and fee in lieu of replacement provisions of

Section 1174.05(c)(2)(B) of the Planning and Zoning Code. As shown on the approved Preliminary Plan, substantial portions of Subarea 1 are reserved for the preservation of existing wooded areas and the Rush Run corridor, and the remaining developable area is constrained by site geometry, infrastructure, and access requirements. Full replacement of natural features within Subarea 1 would result in unreasonable crowding and is not feasible given the site layout depicted on the approved plan. In addition to on-site preservation, the Applicant is committing to the acquisition and long-term preservation of ±6.2 acres of stream corridor, and to the construction of a publicly accessible shared-use path, both of which represent significant environmental and public benefits and involve costs that exceed the applicable fee in lieu of replacement. The proposed development therefore provides a comprehensive approach to natural feature preservation and enhancement consistent with the approved Preliminary Plan. In further consideration of this divergence, the Applicant is committed to provide certain affordable dwelling units reserved for qualified residents.

### **Subarea 2 – Single-Family Transition Lots**

The following development standards apply to Subarea 2, consisting of Lots 1 and 2 of the Planned Unit Development. All development within Subarea 2 shall comply with these standards and the approved Preliminary Plan and associated exhibits.

- A. Design Regulations (Worthington Planning and Zoning Code Section 1174.05(a))
  - 1. Character
    - a. The purpose of Subarea 2 is to provide a buffer and transition between the multifamily development in Subarea 1 and the adjacent single-family residential neighborhood.
  - 2. Design
    - a. Number of Dwelling Units
      - i. A maximum of 2 dwelling units shall be permitted within Subarea 2.
    - b. Building Location and Arrangement
      - i. Minimum Lot Width - 60 feet
      - ii. Minimum Lot Area - 9,000 square feet
      - iii. Minimum Front Yard – 25 feet.
      - iv. Minimum Rear Yard – 25 feet
      - v. Minimum Either Side Yard – 6 feet
      - vi. Minimum Sum of Side Yards – 12 feet
      - vii. Maximum Height of Buildings in Stories – 2-1/2 stories
      - viii. Maximum Height of Buildings in Feet – 30 feet

B. Traffic and Parking

1. Vehicular access and off-street parking within Subarea 2 shall comply with the requirements of Chapter 1171 (Off-Street Parking and Loading) of the Worthington Planning and Zoning Code.
2. Access to the single-family residential lots shall be provided from Park Overlook Drive.

C. Screening and Landscaping

Fencing, landscaping, and screening within Subarea 2 shall comply with Chapter 1180 (Fences, Shrubbery, and Hedges) of the Worthington Planning and Zoning Code.

D. Phasing

The lots may develop in separate phases and independent of Subarea 1 development.



**COMPASS**  
INFRASTRUCTURE GROUP

# Boundless Elford Multifamily Development

445 E Dublin Granville Rd., Worthington, OH

## Traffic Impact Study

Revision 1

June 17<sup>th</sup>, 2026



CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026

2800 Corporate Exchange Drive, Suite 100  
Columbus, OH 43231

## Introduction

This report presents the Traffic Impact Study (TIS) for:

- The proposed Boundless multifamily development located southeast of the intersection of E. Dublin Granville Road and Proprietors Road in Worthington, Ohio.
- The *existing* Boundless campus located west of the multifamily development. It will expand services and add 60 students and 60 employees over the next 10 years. Half will be a student-based program, which currently exists on campus, and the other half will be an adult day program.

The study evaluates traffic operations for the proposed roundabout and the existing signalized intersection at E. Dublin Granville Road (SR 161) and Proprietors Road during the AM and PM peak hours for both the opening year (2026) and the horizon year (2036). The report also includes a qualitative assessment of pedestrian connectivity and potential crossover between the proposed development and adjacent residential areas to the south and west.

### Area Conditions

The proposed multifamily development is located at 445 E. Dublin Granville Road (SR 161) in Worthington, Ohio, southeast of the intersection of SR 161 and Proprietors Road. Site access is proposed *only* via the existing Boundless property access, which connects to SR 161 via the south leg of the Proprietors Road intersection.

SR 161 in this area is a three-lane east–west principal arterial consisting of one through lane in each direction and a center two-way left-turn lane. The posted speed limit is 35 mph. Proprietors Road is a two-lane road that serves as a major collector to the north and a local road to the south, with a posted speed limit of 35 mph. The south leg of the intersection serves as access to the Boundless property. This intersection is part of a coordinated signal system along SR-161, integrated with both the Dublin–Granville Road and High Street corridors.

The proposed development consists of 246 low-rise multifamily residential units on approximately 19.9 acres. The site plan and additional development details are provided in *Appendix A*. The Boundless expansion discussed in the Introduction is also included in all analyses.

### Scope of Study

In coordination with the City of Worthington, the following study elements were selected for evaluation:

1. Existing signalized intersection of SR-161 and Proprietors Road
2. Proposed roundabout on Proprietors Road within the Boundless site, which will provide primary access to the proposed development
3. Pedestrian connectivity and potential crossover between the proposed development, the Boundless site, and adjacent residential areas

### Traffic Volumes and Conditions

A 12-hour turning movement count was conducted at the intersection of SR-161 and Proprietors Road on December 17, 2025, by Quality Counts, LLC. Linear annual growth rates for each approach were obtained from MORPC based on the collected traffic data. The turning movement count data and corresponding growth rate information are provided in *Appendix B*.

## Analysis

### Background Traffic

The opening year and horizon year for the analysis are 2026 and 2036, respectively. AM and PM peak-hour background (No Build) traffic volumes for both analysis years were developed by applying MORPC-provided linear annual growth rates to the existing turning movement count data.

### Trip Generation

Trip generation for the proposed multifamily development was estimated using the *Institute of Transportation Engineers (ITE) Trip Generation Manual*, 12th Edition. Land Use Code (LUC) 220 – Multifamily Housing (Low-Rise) was utilized for the analysis. This is typical, accepted national practice for this type of analysis. The resulting AM and PM peak hour trip generation summaries are provided in *Appendix C*.

The additional trips generated by the Boundless campus were estimated separately because there is no ITE land-use code that directly represents the proposed Boundless operation, which is unique and includes a mix of student-based programs and adult-day programs. Therefore, campus trip generation was estimated using the existing intersection counts collected at the Worthington Boundless campus, with adjustments based on existing and proposed campus operations:

The existing Worthington campus includes approximately 60 students and 150 employees, for a total of 210 people. Boundless confirmed that approximately 60 employees are directly associated with the existing student-based program of 60 students. Therefore, the existing student-based program represents approximately 120 of the 210 total people on campus, or 57% of the existing campus activity.

The proposed Boundless expansion includes 60 additional students and 60 additional employees split evenly between the student-based program and the adult-day program. The adult-day program will operate from approximately 9:30 AM to 3:00 PM with students arriving and departing by small bus or van. Since the adult-day student trips are expected to occur outside the study peak hours, the 30 adult-day students are not included in the peak-hour trip-generation estimate. Additionally, at most half of the associated 30 adult-day employees are expected to contribute to peak-hour traffic. Therefore, 75 of the 120 proposed students and employees are expected to contribute to peak-hour traffic, resulting in an adjustment factor of 0.62.

Based on these assumptions, the proposed Boundless campus peak-hour trips were estimated as follows:  
-Existing Campus Peak Hour Trips  $\times 0.57 \times 0.62 = 0.35 \times$  Existing Campus Peak Hour Trips

### Trip Distribution

Site-generated traffic was distributed and assigned based on existing traffic patterns observed in the peak-hour turning movement count data (see *Appendix D*) and applied to the signalized intersection at SR-161 and Proprietors Road.

For the proposed roundabout, traffic was assigned by separating existing Boundless traffic from site-generated traffic. Existing Boundless traffic, as reflected in the turning movement count data was assigned to the south and west legs of the roundabout with an assumed 50/50 distribution, while the development site-generated traffic was assigned to the east leg. This approach assumes no traffic interaction between Boundless and the proposed development.

The following traffic scenarios were developed for analysis:

- **Opening Year No Build (2026):** Existing traffic volumes with applied background growth
- **Opening Year Build (2026):** Opening Year No Build volumes with development site-generated traffic and the Boundless expansion traffic added

- **Horizon Year No Build (2036):** Opening Year No Build volumes projected to the horizon year
- **Horizon Year Build (2036):** Horizon Year No Build volumes with development site-generated traffic and the Boundless expansion traffic added

## Intersection Operational Analysis

Operational analysis for the signalized intersection of SR-161 and Proprietors Road and the proposed single-lane roundabout was performed using Highway Capacity Software (HCS 2025, Version 8.4), consistent with methodologies outlined in the *Highway Capacity Manual (HCM), 7th Edition*.

During the AM and PM peak hours, a Level of Service (LOS) D or better is the standard, accepted threshold for overall intersection operations, with LOS E considered acceptable for individual movements. The proposed signal timings from the Worthington Signal Corridor Study were used to analyze both the Build and No-Build conditions. Signal timing optimization was performed while maintaining the existing cycle length and offsets to preserve system coordination, and the results were compared across scenarios.

## Pedestrian Cross-over Analysis

Pedestrian activity has been observed between the existing Boundless development and adjacent neighborhoods. This study documented the probable points of these existing pedestrian connections and identified potential future pedestrian connection points. A qualitative assessment was conducted to evaluate how the proposed development may affect existing pedestrian activity on the Boundless property and in the adjacent neighborhood.

# Results

## Capacity Analysis

The results of the capacity analysis are summarized in *Table 1*. During the AM peak hour, the intersection operates at acceptable LOS under all scenarios.

During the PM peak hour, the intersection operates at LOS E under the proposed signal timing for both the Opening Year and Horizon Year, with and without site traffic. This does not meet the established LOS criteria. The eastbound and westbound through movements also operate at LOS F during the PM peak hour, Horizon Year, Build and No Build conditions. Unacceptable LOS values are highlighted in red in *Table 1*.

To evaluate potential operational improvements, the signal timing was optimized keeping in mind that the intersection is coordinated with other intersections on the corridor. Both cycle length and offsets were kept the same as the timings from the Worthington signal study report (see *Appendix E Section 1*). The optimized results are presented in the "Build Opt." column in *Table 1*. With timing optimization only, the overall intersection operations improved during both the AM and PM peak hours to acceptable levels. Under horizon year full-build PM peak conditions, the overall intersection LOS improved from LOS E to LOS C, with the average intersection delay reduced from 73 seconds per vehicle to 33 seconds per vehicle. The eastbound through movement improved from LOS F to LOS C, and the westbound through movement improved from LOS F to LOS D.

With signal optimization, all movements operate with acceptable LOS during both AM and PM peak hours under the opening-year and horizon-year build conditions.

All analysis output is provided in *Appendix E*.

Table 1: Signalized LOS Summary (LOS/Seconds of Delay)

SR-161 & Proprietors Rd. (Signalized)	Approach/ Movement	AM					
		Opening Yr. (2026)			Horizon Yr. (2036)		
		No-Build	Build	Build Opt.	No-Build	Build	Build Opt.
	EBL	B/17.2	B/17.2	B/10.9	B/18.5	B/18.5	B/12.6
	EBT	C/29.7	C/31.0	B/17.4	C/31.4	C/32.9	B/19.5
	WBL	B/13.5	B/15.1	A/9.4	B/14.2	B/16.0	B/10.7
	WBT	D/37.5	D/37.5	B/18.7	D/42.1	D/42.1	C/21.5
	NB	C/28.6	C/31.6	D/35.3	C/28.6	C/31.6	C/33.9
	SB	D/36.8	D/37.6	D/39.4	D/38.4	D/39.4	D/38.7
	<b>Intersection</b>	<b>C/32.5</b>	<b>C/32.7</b>	<b>C/20.7</b>	<b>D/35.3</b>	<b>D/35.5</b>	<b>C/22.6</b>
		PM					
	EBL	B/18.5	B/18.5	B/16.9	B/18.5	B/18.5	C/20.5
	EBT	D/49.9	<b>E/55.6</b>	C/24.0	<b>F/59.4</b>	<b>F/66.1</b>	C/29.5
	WBL	B/16.7	B/19.0	B/13.8	B/17.5	B/19.0	B/16.4
	WBT	<b>F/82.0</b>	<b>F/82.0</b>	C/27.7	<b>F/98.3</b>	<b>F/98.3</b>	D/37.1
	NB	C/31.9	C/34.8	D/35.6	C/32.0	C/35.0	C/34.2
	SB	D/43.3	D/43.0	D/39.5	D/46.3	D/46.1	D/38.8
	<b>Intersection</b>	<b>E/61.9</b>	<b>E/62.0</b>	<b>C/27.2</b>	<b>E/73.0</b>	<b>E/73.0</b>	<b>C/33.2</b>

### Roundabout Analysis

Consistent with the Memorandum of Understanding with the City, roundabout operations were evaluated for the Horizon Year Build condition (2036) during the AM and PM peak hours.

The results, summarized in *Table 2*, indicate that the proposed single-lane roundabout operates at LOS A during both peak periods for the Horizon Year Build condition, which represents the highest-volume scenario analyzed. Based on these results, the roundabout will operate at LOS A or better under all other analysis scenarios.

Table 2: Roundabout LOS Summary (LOS/Seconds of Delay)

Proposed 1x1 Roundabout	Approach	Horizon Yr. Build (2036)	
		AM	PM
	EB	A/3.2	A/3.1
WB	A/3.3	A/3.2	
NB	A/3.0	A/3.1	
SB	A/3.8	A/3.2	
<b>Intersection</b>	<b>A/3.6</b>	<b>A/3.2</b>	



## Pedestrian Cross-over Analysis

The site has been visited on five separate occasions since October 2025. The most recent field review occurred in March under conditions highly conducive to pedestrian activity, including observable foot traffic from nearby athletic fields that traveled through adjacent neighborhoods (not crossing into Boundless). Based on these field observations and supplemental research, the following was noted:

- Based on public input and reported behavior, the existing pedestrian attractor is the Boundless campus, with neighborhood residents utilizing the low-volume Boundless roads for recreational walking.
- Multiple opportunities exist for both confident and less-confident pedestrians to traverse between the existing Boundless campus and adjacent neighborhoods. These include a gated emergency access drive and informal connections across grassed and lightly wooded areas. These represent informal, non-designated pedestrian connections.
- Across approximately three hours of cumulative observation time, no pedestrian crossover between the Boundless campus and adjacent neighborhoods was observed.
- The Boundless campus is not a secured facility, and some residents can move freely. While pedestrian crossover is therefore possible under existing conditions, it was not observed during field reviews.

Pedestrian activity is fundamentally driven by the presence of an attractor. Based on verbal reports and public input to the City, existing pedestrian interaction appears to be recreational, with neighborhood residents using the Boundless campus for its low-volume, low-speed roadway environment. Based on public input and reported behavior, the current attractor creating crossover is the Boundless campus, creating flow from the neighborhood to Boundless.

While future residents of the proposed development may also find the remaining campus roadway network suitable for recreational walking, this does not inherently create cross-property pedestrian demand. For meaningful crossover between the development and surrounding neighborhoods to occur, a defined destination or attractor must exist.

Public comments have suggested that retail and social destinations along High Street may serve as such an attractor. However, the closest destinations are approximately 0.9 miles from the proposed development via the most direct route, which is a 20-minute one-way walk, or roughly 1.8 miles (40 minutes) round trip. National planning data consistently indicates that typical acceptable pedestrian walking distances range from 0.25 to 0.50 miles. As such, a round trip to the High Street corridor is well outside the typical pedestrian walkshed and is not expected to function as a meaningful attractor for pedestrian trips.

It could be argued that residents of the proposed development may instead be attracted to walk within adjacent neighborhood streets. However, existing behavior indicates that some neighborhood residents currently choose to walk within the Boundless campus rather than on neighborhood streets, suggesting a preference for the campus environment. It is therefore reasonable to expect that future residents of the development would exhibit similar behavior. This is further supported by limited sidewalk connectivity in adjacent neighborhoods, including streets without sidewalks, which reduces pedestrian comfort compared with the Boundless campus roadway network or the sidewalks along SR-161 and Proprietors Road.

The one key attractor is the existing elementary school. It is nearly 0.5 miles from the proposed development at its closest point. It's roughly 0.7 miles from its furthest point. This distance is at or beyond the upper limit of typical elementary school walking distances, particularly given incomplete sidewalk connectivity and indirect routing. As a result, walking is expected to be limited, with school bus service or parental drop-off likely to be the primary modes of access.

## Conclusions & Recommendations

- *Table 1* shows that after signal optimization, Build and No Build results show improved operations over the existing intersection operations.
- For Proprietors Rd. & SR-161, no operational issues were identified during the AM peak hour in either the opening year or the horizon year.
- During the PM peak hour, signal optimization is recommended to improve LOS for all approaches once buildout and occupancy occur.
- For the single-lane roundabout, no operational issues are expected under any conditions.
- There is limited evidence of a pedestrian-attractor that would result in a measurable increase in pedestrian crossover between the proposed development to the adjacent neighborhoods. While the nearby elementary school may contribute to a minor increase in pedestrian activity, any such increase is expected to be minimal.



**LEGAL DESCRIPTION**  
**20.0± ACRE**  
**ZONING BOUNDARY (SUBAREA-1)**

Situated in the State of Ohio, County of Franklin, City of Worthington, being part of Quarter Township 3, Township 2, Range 18, United States Military Lands, beings parts of those two parcels of land (First Parcel and Second Parcel) as described in a deed to The Columbus Sanitarium Company (n.k.a. Harding Sanitarium, Inc.), of record in Deed Volume 874, Page 274, all references herein being to the records of the Recorder's Office, Franklin County, Ohio, and being more particularly described as follows:

**COMMENCING** at the northwesterly corner of the Second Parcel, at the northeasterly corner of that 2.25 acre tract as described in a deed to Harding Sanitarium, Inc., of record in Deed Volume 1885, Page 81 and in the centerline of Dublin-Granville Road (State Route 161);

Thence South 86°51'52" East, along said centerline, a distance of 71.28 feet to the intersection of centerline of Proprietors Road, 50 feet in width, being also a corner common to the First Parcel and Second Parcel;

Thence South 86°55'27" East, continuing along said centerline, a distance of 403.39 feet to the northeasterly corner of the First Parcel and in the westerly right-of-way line of the Norfolk & Western Railway Co.;

Thence South 03°21'12" East, along said right-of-way line, a distance of 300.00 feet to a point in the easterly line of the 40 foot wide (+/-) strip of land (originally out of said First Parcel) as described in a deed to Ohio Railway Museum, Inc., of record in Official Record Volume 29706, Page E09;

Then northerly along said easterly line the following two courses:

- 1) North 13°18'55" West, a distance of 24.09 feet to a point of curvature;
- 2) With the arc of a curve to the right, having a radius of 1753.18 feet, a central angle of 08°31'54", a chord that bears North 09°02'57" West, a chord distance of 260.82 feet to a point in the original southerly right-of-way line of Dublin-Granville Road (20.00 feet south of centerline);

Thence North 86°55'27" West, along said right-of-way line, a distance of 44.77 feet to a point at the intersection of the westerly line of said 40 foot wide (+/-) strip;

Thence southerly along said westerly line the following courses:

- 1) South 06°52'23" East passing an iron pin set in the southerly right-of-way line of Dublin-Granville Road at 35.53 feet, a total distance of 117.77 feet to a point being the **TRUE POINT OF BEGINNING**;

**BEGINNING** at a point along said westerly line right-of-way line of Norfolk & Western Railway Co. as described in a deed to Ohio Railway Museum, Inc., of record in Official Record Volume 29706, Page E09;

Thence southerly along said westerly line the following courses:

- 1) South 06°52'28" East, a distance of 51.59 feet to a point;
- 2) South 13°12'28" East, a distance of 819.40 feet to an iron pin set at the point of curvature;
- 3) With an arc of a curve to the right, having a radius of 5699.00 feet, a central angle of 09°59'57", a chord that bears South 08°12'29" East, a chord distance of 993.32 feet to a point at the northeasterly corner of the subdivision "Colonial Hills No. 2", of record in Plat Book 20, Page 10, in the easterly right-of-way line of a vacated alley;

Thence North 86°21'34" West, along the northerly line of Colonial Hills No. 2, a distance of 140.23 feet to a point;

Thence North 02°51'05" West, a distance of 30.19 feet to a point;



Thence North 86°21'34" West, along the northerly line of a 0.402 acre tract (northerly right-of-way line of Park Overlook) as described in a deed to The Board of Commissioners of Franklin County, Ohio (for right-of-way purposes), of record in Deed Volume 1688, Page 284, a distance of 113.59 feet to a point;

Thence North 02°54'50" East, a distance of 151.53 feet to a point;

Thence North 87°05'10" West, passing the northeasterly corner of the property of Karen S. and Shawn E. Miller, of record instrument No. 200103090048575, a distance of 120.00 feet to a point;

Thence northerly the following courses:

- 1) North 86°21'34" West, a distance of 128.07 feet to a point;
- 2) North 02°17'01" East, a distance of 179.71 feet to a point;
- 3) North 87°46'20" West, a distance of 40.91 feet to a point;
- 4) With an arc of a curve to the right, having a radius of 126.71 feet, a central angle of 36°33'33", a chord that bears North 68°46'27" West, a chord distance of 79.48 feet to a point;
- 5) With an arc of a curve to the left, having a radius of 116.50 feet, a central angle of 76°00'45", a chord that bears North 25°29'27" East, a chord distance of 143.47 feet to a point;
- 6) North 14°14'39" West, a distance of 153.19 feet to a point;
- 7) With an arc of a curve to the right, having a radius of 350.00 feet, a central angle of 26°32'26", a chord that bears North 00°58'25" West, a chord distance of 160.68 feet to a point;
- 8) North 12°17'48" East, a distance of 85.36 feet to a point;
- 9) With an arc of a curve to the left, having a radius of 219.10 feet, a central angle of 48°50'59", a chord that bears North 12°29'39" West, a chord distance of 181.20 feet to a point;
- 10) North 01°41'11" West, a distance of 173.00 feet to a point;
- 11) With an arc of a curve to the right, having a radius of 88.50 feet, a central angle of 47°00'26", a chord that bears North 43°50'56" West, a chord distance of 70.59 feet to a point;

Thence westerly the following courses:

- 1) South 76°05'04" West, a distance of 213.57 feet to a point;
- 2) South 59°40'09" West, a distance of 292.80 feet to a point;
- 3) South 65°51'58" West, a distance of 358.51 feet to a point along the easterly line of "Rush Creek Subdivision" (a re-plat of a portion of Subdivision of Reserve "B", Colonial Hills Plat No. 2) of record in Plat Book 28, Pages 38 and 39);

Thence North 02°57'00" East, along the easterly line of said Rush Creek Subdivision, a distance of 281.05 feet to an iron pin set at the southwesterly corner of the 0.69 acre tract as described in a deed to George T. and Joan L Harding, of record in Deed Volume 2316, Page 198;

Thence South 86°58'28" East, along the southerly line of said 0.69 acre tract, a distance of 200.03 feet to an iron pin set at the southeasterly corner of same and at the southwesterly corner of that 2.323 acre tract as described in a deed to Harding Hospital, Inc., of record in Deed Volume 3790, Page 425;

Thence North 58°27'44" East, along the southerly line of said 2.323 acre tract, a distance of 534.25 feet to an iron pin set at the most easterly corner of same and at the southeasterly corner of the aforementioned 2.25 acre tract;

Thence North 78°37'05" East, a distance of 174.01 feet to a point;

Thence North 41°57'57" East, a distance of 323.63 feet to a point;

Thence North 80°19'43" East, a distance of 52.45 feet to the TRUE POINT OF BEGINNING, containing +/- 20.0 acres more or less.

This description is based on a field survey conducted on March 9, 2026.



**LEGAL DESCRIPTION**  
**0.4± ACRE**  
**ZONING BOUNDARY (SUBAREA-2)**

Situated in the State of Ohio, County of Franklin, City of Worthington, being part of Quarter Township 3, Township 2, Range 18, United States Military Lands, being part of an original 40.934 acre tract of land, referred to as "Tract 1" conveyed to Strategic Resources, Inc. by deed in Instrument Number 201402270024366 and Affidavits in Instrument Number 201910080133085 and Instrument Number 202505050045860, all records being of the Recorder's Office, Franklin County, Ohio and being more particularly bounded and described as follows: all references herein being to the records of the Recorder's Office, Franklin County, Ohio, and being more particularly described as follows:

**BEGINNING** at a point on the northerly right-of-way line of Park Overlook Drive (60 feet wide), also being on the northerly line of a 0.402 acre tract of land conveyed to The Board of County Commissioners of Franklin County in D.B. 1688, Page 284, also being a southerly corner of said original 40.934 acre tract, and also being the the southeasterly corner of a 0.0688 acre tract of land, referred to as "PARCEL TWO", conveyed to KAREN S. MILLER AND SHAWN E. MILLER in Instrument Number 200103090048575;

Thence along a westerly line of said original 40.934 acre tract, North 02°54'50" East, 150.01 feet to a point at a southerly corner of said original 40.934 acre tract;

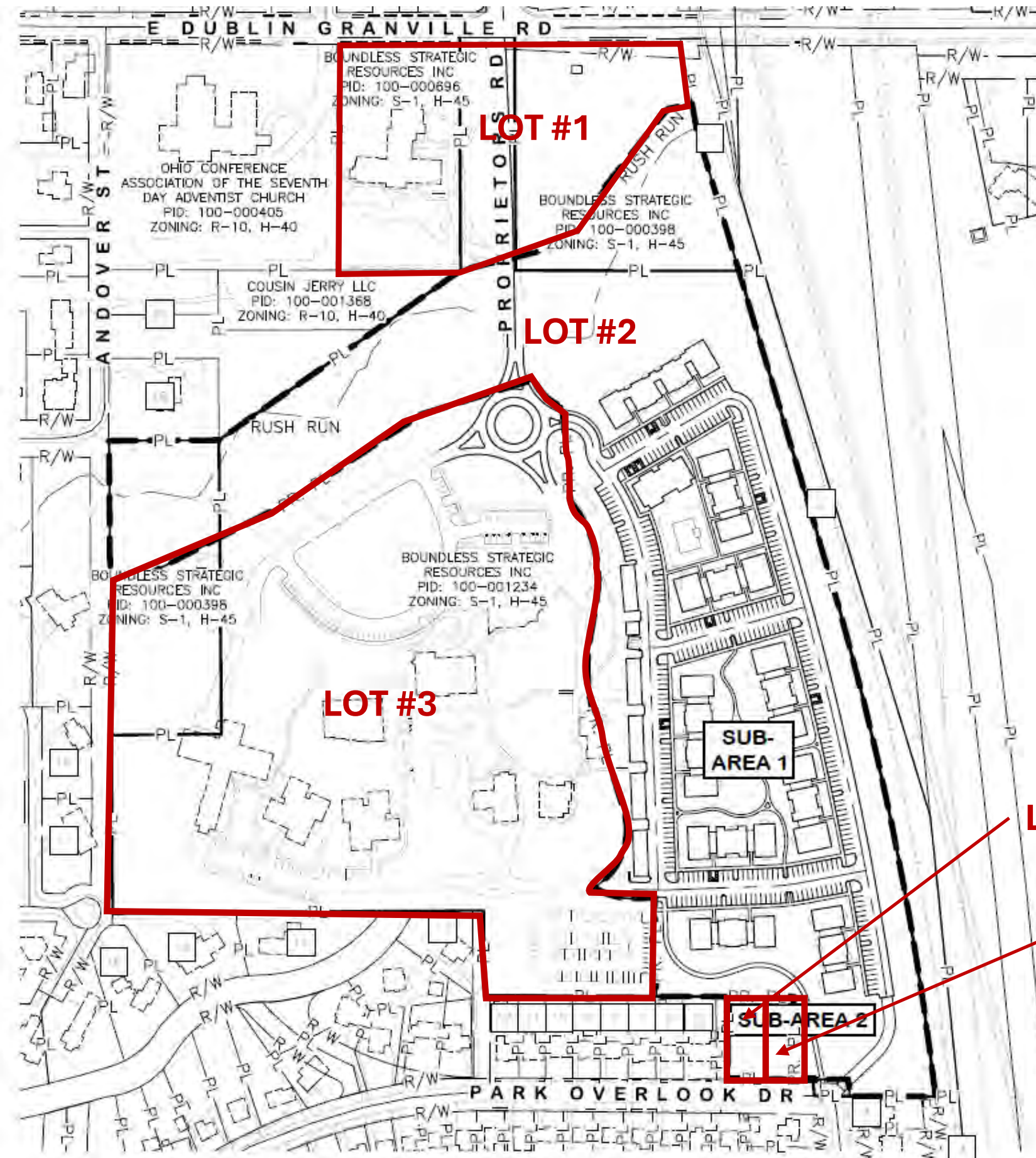
Thence across said original 40.934 acre tract for the following two (2) courses:

- 1) South 87°05'10" East, 120.00 feet to a point;
- 2) South 02°54'50" West, 151.53 feet to a point on the southerly line of said original 40.934 acre tract;

Thence along said southerly line, North 86°21'34" West, 120.01 feet to the **TRUE POINT OF BEGINNING**, containing 0.4± acres, more or less.

This description is based on a field survey conducted on March 9, 2026.





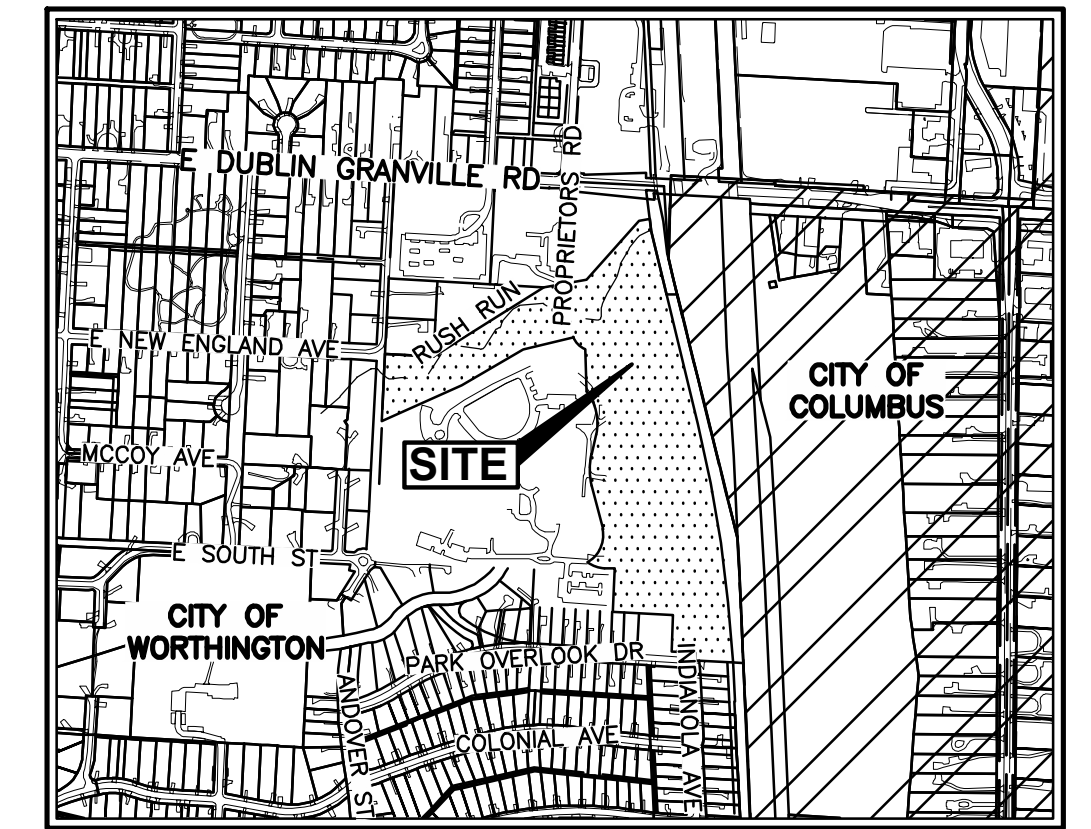
# PRELIMINARY SITE PLAN

# BOUNDLESS

## ELFORD DEVELOPMENT

## CITY OF WORTHINGTON, OHIO

# 2026



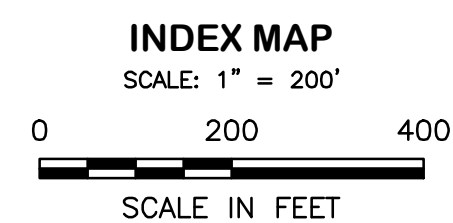
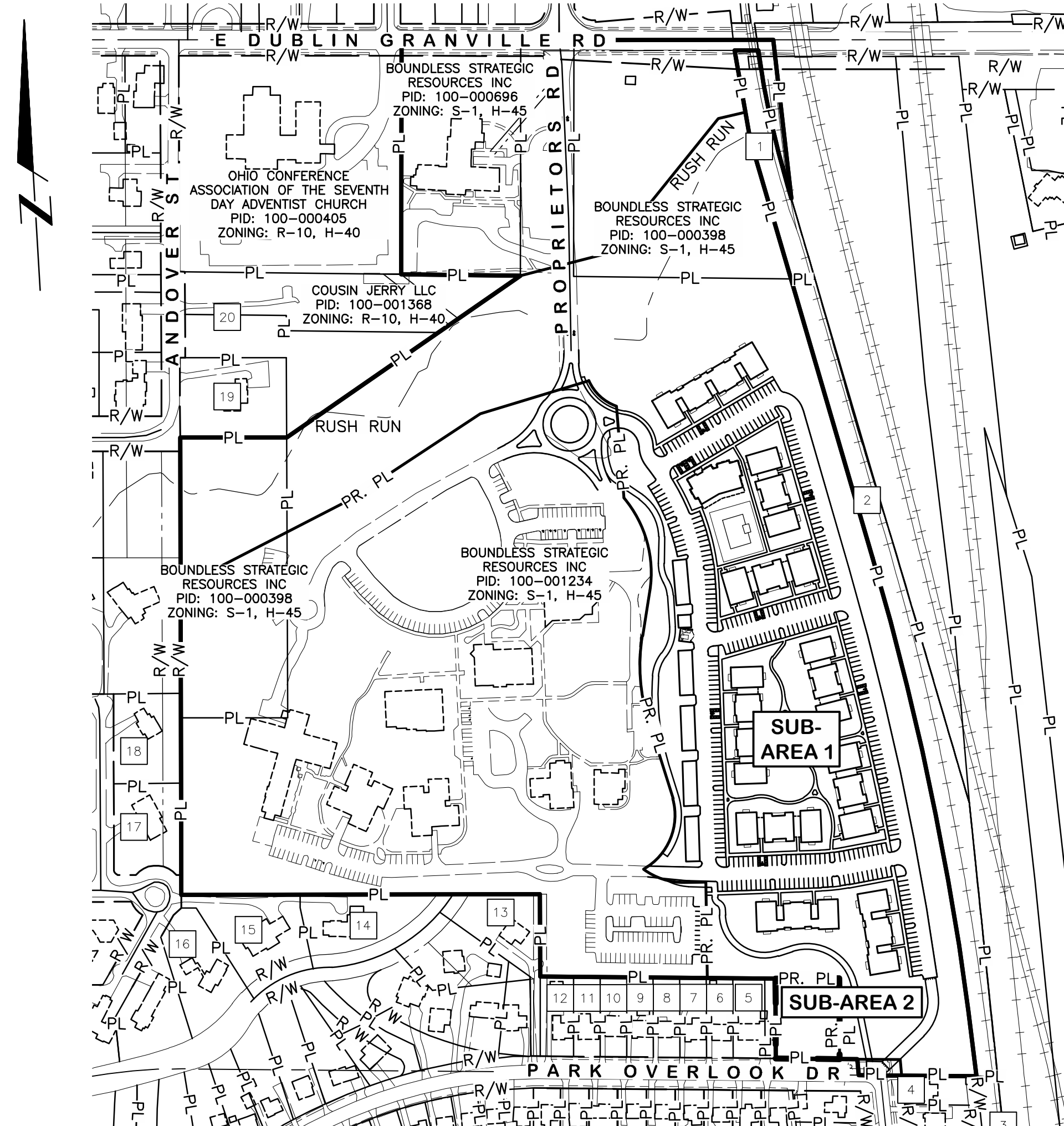
**LOCATION MAP**  
NOT TO SCALE

SITE DATA TABLE		
DESCRIPTION	QUANTITY	UNIT
TOTAL SITE AREA (PRIVATE)	20.01	AC
TOTAL DISTURBED AREA	11.22	AC
PRE-DEVELOPED IMPERVIOUS AREA	3.19	AC
POST-DEVELOPED IMPERVIOUS AREA	6.95	AC

GENERAL ZONING INFORMATION	
MULTI-FAMILY (SUB-AREA 1)	
SITE ADDRESS	445 E DUBLIN GRANVILLE RD
PARCEL NUMBER(S)	100-001234 & 100-000398
EXISTING ZONING CLASSIFICATION/DISTRICT	S-1
PROPOSED USE	MULTI-FAMILY RESIDENTIAL
HEIGHT DISTRICT	H-45
TOTAL SITE AREA (ACRES)	20.01
FLOOD INSURANCE RATE MAP NUMBER (FIRM)	39049C0159K
MOST RECENT EFFECTIVE DATE OF FIRM	06/17/2008
BASE FLOOD ELEVATION (BFE)	N/A
SINGLE FAMILY (SUB-AREA 2)	
NUMBER OF LOTS	2
TOTAL AREA (ACRES)	0.41
LOT FRONTAGE	60'
FRONT SETBACK	25'
REAR SETBACK	25'
SIDE SETBACK	6'

GENERAL PROJECT INFORMATION	
MULTI-FAMILY (SUB-AREA 1)	
MAXIMUM BUILDING HEIGHT	45'
PROPOSED BUILDING HEIGHT	41'-3"
NUMBER OF DWELLING UNITS (RESIDENTIAL ONLY)	246
DENSITY	12.3 UNITS/AC
TOTAL PROPOSED PARKING	356

- 1 OHIO RAILWAY MUSEUM INC  
PID: 100-000529
- 2 OHIO RAILWAY MUSEUM INC  
PID: 100-001239
- 3 OHIO RAILWAY MUSEUM INC  
PID: 100-001998
- 4 MILLER KAREN S  
PID: 100-006074  
ZONING: R-10, H-40
- 5 MILLER KAREN S  
PID: 100-002108  
ZONING: R-10, H-40
- 6 SHUMWAY MARTIN R  
PID: 100-001478  
ZONING: R-10, H-40
- 7 SHUMWAY MARTIN R  
PID: 100-001479  
ZONING: R-10, H-40
- 8 WARREN MARK D  
PID: 100-001399  
ZONING: R-10, H-40
- 9 BARTELT WILLIAM F III  
PID: 100-001485  
ZONING: R-10, H-40
- 10 DEWINE AMANDA  
PID: 100-001476  
ZONING: R-10, H-40
- 11 SUTHERLAND LAURIE A  
PID: 100-001477  
ZONING: R-10, H-40
- 12 SMULLEN JAKE  
PID: 100-001480  
ZONING: R-10, H-40
- 13 HOSTER BARRET B  
PID: 100-001348  
ZONING: R-10, H-40
- 14 SEITZ BRIAN A  
PID: 100-002278  
ZONING: R-10, H-40
- 15 TOMKO CAROLE W  
PID: 100-002277  
ZONING: R-10, H-40
- 16 WILLIAMS REGINALD A  
PID: 100-002276  
ZONING: R-10, H-40
- 17 CHANG LIYAN DAVID  
PID: 100-002272  
ZONING: R-10, H-40
- 18 DOTSON JEFFREY  
PID: 100-002268  
ZONING: R-10, H-40
- 19 ARNETT CHRISTOPHER ALLEN  
PID: 100-001079  
ZONING: R-10, H-40
- 20 COUSIN JERRY LLC  
PID: 100-000780  
ZONING: R-10, H-40



**SHEET INDEX**

TITLE .....	1
EXISTING CONDITIONS .....	2
PRELIMINARY ZONING PLAT .....	3
SITE LAYOUT PLAN .....	4
SITE UTILITY PLAN .....	5
PRELIMINARY GRADING PLAN .....	6

**DEVELOPER INFORMATION:**  
ELFORD DEVELOPMENT  
1220 DUBLIN ROAD  
COLUMBUS, OHIO 43125  
CONTACT: JOE GAVIN  
PHONE: (614) 581-3945  
EMAIL: jgavin@elford.com

**OWNER INFORMATION:**  
ELFORD DEVELOPMENT  
1200 DUBLIN ROAD  
COLUMBUS, OHIO 43215  
CONTACT: MIKE FITZPATRICK  
PH: (614) 488-0000  
EMAIL: mfitzpatrick@elford.com

**ARCHITECT INFORMATION:**  
ARCHALL ARCHITECTS  
49 E THIRD AVENUE  
COLUMBUS, OHIO, 43201  
CONTACT: BRAD PARISH  
PHONE: (614) 469-7500  
EMAIL: bparish@archall.com

**CIVIL ENGINEER INFORMATION:**  
E.P. FERRIS & ASSOCIATES  
2130 QUARRY TRAILS DRIVE, 2ND FLOOR  
COLUMBUS, OHIO 43228  
CONTACT: JUSTIN A. RAY, P.E.  
PHONE: (614) 299-2999  
FAX: (614) 299-2992  
EMAIL: jray@epferris.com

M:\1050030\_Boundless\DWG\Production Drawings\1050030\_Preliminary\Title.dwg ~Title\_PLOTTED BY:RAY ON 05/14/2026 11:19



2130 QUARRY TRAILS DR,  
2ND FLOOR  
COLUMBUS, OHIO 43228  
(614) 299-2999  
(614) 299-2992 (Fax)  
www.EPFERRIS.com

**PRELIMINARY**  
**NOT FOR**  
**CONSTRUCTION**

BY \_\_\_\_\_ REGISTERED ENGINEER DATE \_\_\_\_\_

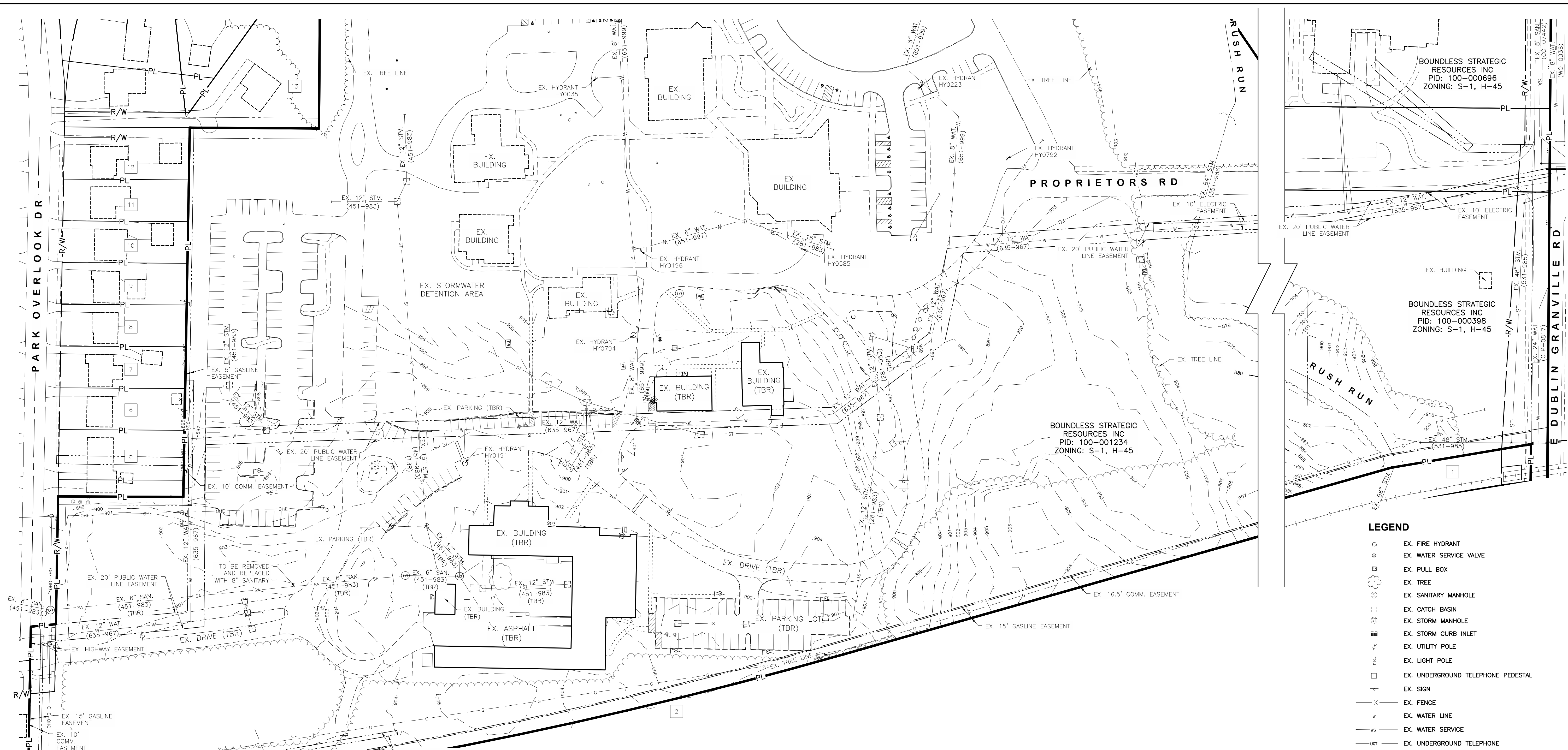


**800-362-2764 or 8-1-1**  
[www.oups.org](http://www.oups.org)

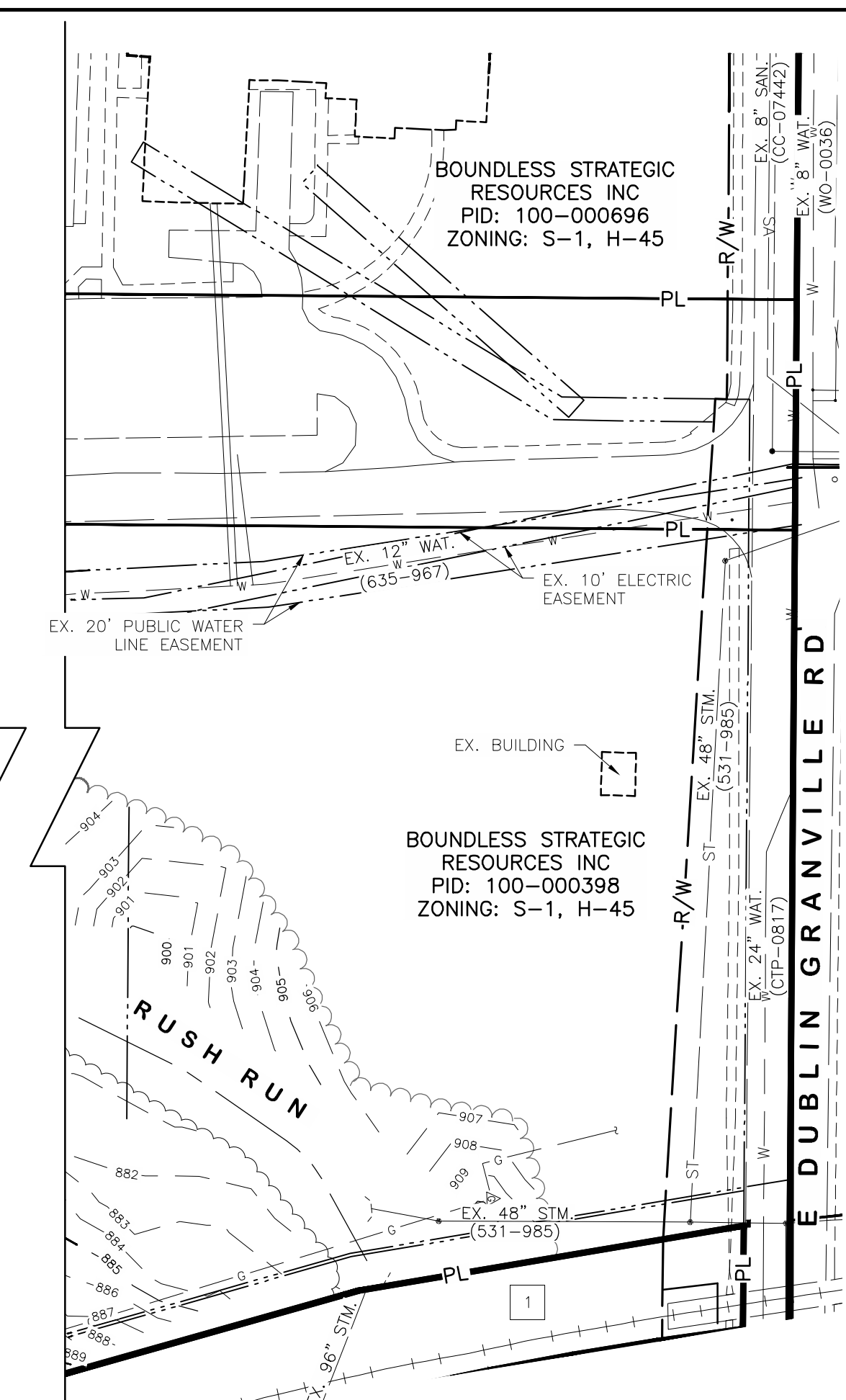
CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026

SHEET NO.	OF
1	6

W:\1050030\_Boundless\DWG\Production Drawings\1050030\_Preliminary\Existing Conditions\1050030\_Plot\1050030\_Plot.dwg - Existing Conditions PLOTTED BY:RAY ON 05/14/2026 11:19



- |   |   |    |  |
|---|---|----|--|
| 1 | OHIO RAILWAY MUSEUM INC<br>PID: 100-000529                | 8  | WARREN MARK D<br>PID: 100-001399<br>ZONING: R-10, H-40         |
| 2 | OHIO RAILWAY MUSEUM INC<br>PID: 100-001239                | 9  | BARTELT WILLIAM F III<br>PID: 100-001485<br>ZONING: R-10, H-40 |
| 3 | OHIO RAILWAY MUSEUM INC<br>PID: 100-001998                | 10 | DEWINE AMANDA<br>PID: 100-001476<br>ZONING: R-10, H-40         |
| 4 | MILLER KAREN S<br>PID: 100-006074<br>ZONING: R-10, H-40   | 11 | SUTHERLAND LAURIE A<br>PID: 100-001477<br>ZONING: R-10, H-40   |
| 5 | MILLER KAREN S<br>PID: 100-002108<br>ZONING: R-10, H-40   | 12 | SMULLEN JAKE<br>PID: 100-001480<br>ZONING: R-10, H-40          |
| 6 | SHUMWAY MARTIN R<br>PID: 100-001478<br>ZONING: R-10, H-40 | 13 | HOSTER BARRET B<br>PID: 100-001348<br>ZONING: R-10, H-40       |
| 7 | SHUMWAY MARTIN R<br>PID: 100-001479<br>ZONING: R-10, H-40 |    |  |



- LEGEND**
- EX. FIRE HYDRANT
  - EX. WATER SERVICE VALVE
  - EX. PULL BOX
  - EX. TREE
  - EX. SANITARY MANHOLE
  - EX. CATCH BASIN
  - EX. STORM MANHOLE
  - EX. STORM CURB INLET
  - EX. UTILITY POLE
  - EX. LIGHT POLE
  - EX. UNDERGROUND TELEPHONE PEDESTAL
  - EX. SIGN
  - EX. FENCE
  - EX. WATER LINE
  - EX. WATER SERVICE
  - EX. UNDERGROUND TELEPHONE
  - EX. GAS
  - EX. STORM
  - EX. SANITARY
  - EX. UNDERGROUND ELECTRIC
  - EX. OVERHEAD ELECTRIC
  - EX. RIGHT OF WAY
  - EX. PROPERTY LINE
  - SUBJECT PROPERTY LINE
  - EX. MINOR CONTOUR ELEVATION
  - EX. MAJOR CONTOUR ELEVATION
  - TO BE REMOVED

CITY OF WORTHINGTON  
 DRAWING NO. PUD 01-2026  
 DATE 06/17/2026

SCALE: 1" = 60'  
 SHEET NO. 2 OF 6

REVISIONS	BY	DATE

**E. P. FERRIS**  
 AND  
**ASSOCIATES**  
 Consulting Civil Engineers and Surveyors

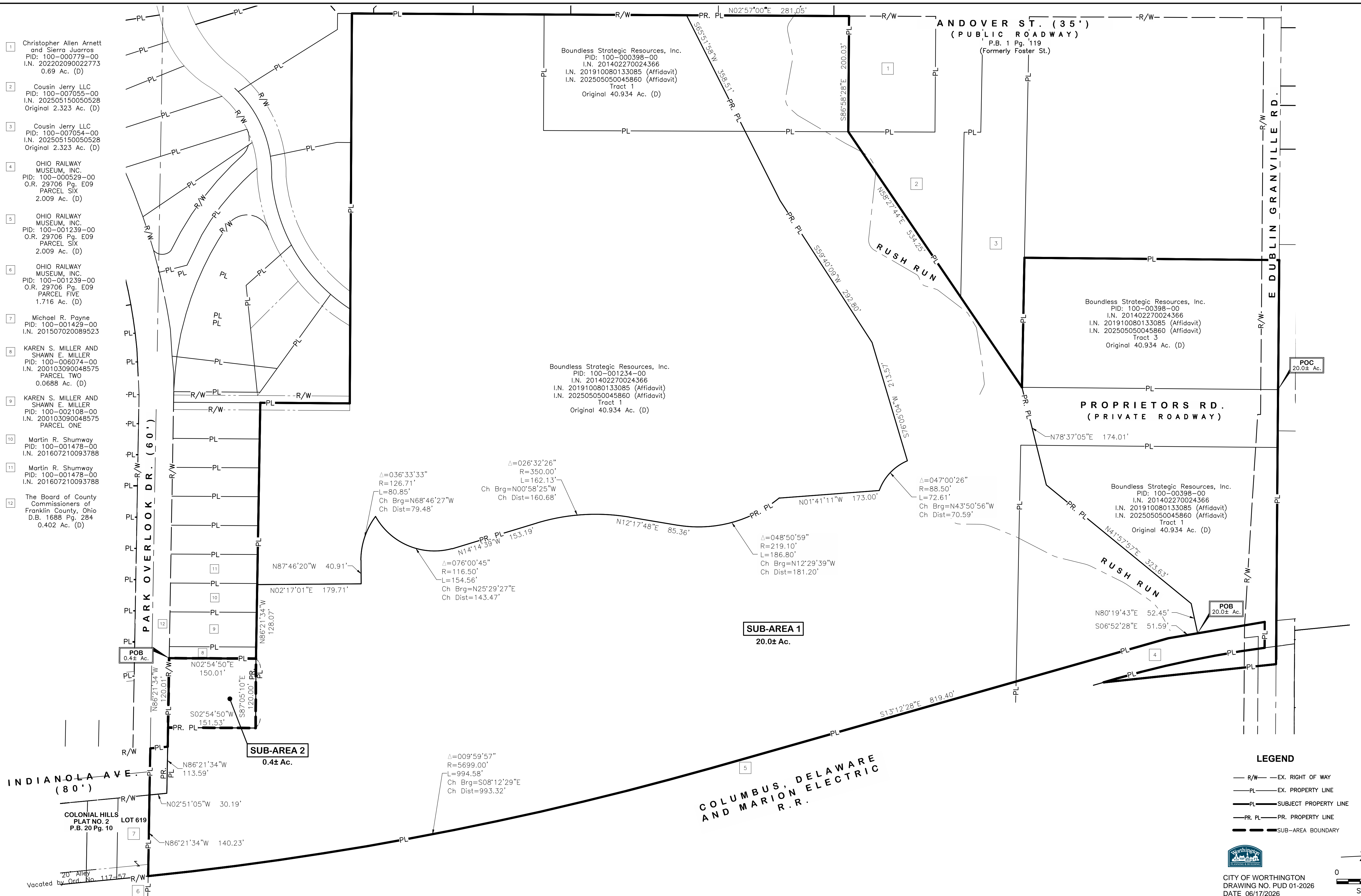
2130 QUARRY TRAILS DR,  
 2ND FLOOR  
 COLUMBUS, OHIO 43228  
 (614) 299-2999  
 (614) 299-2992 (Fax)  
 www.EPFERRIS.com

CITY OF WORTHINGTON, OHIO  
**BOUNDLESS**  
 ELFORD DEVELOPMENT

JOB NO.:	1050.030
DESIGNED BY:	SKD
DRAWN BY:	SKD
CHECKED BY:	JAR
APPROVED BY:	CLP
DATE:	5/15/2026

**EXISTING CONDITIONS**

W:\1050030\_Boundless\DWG\Production Drawings\1050030\_Preliminary Zoning Plat.dwg --Preliminary Zoning Plat.dwg --PLOTTED BY:RAY ON 05/14/2026 11:20



- 1 Christopher Allen Arnett and Sierra Juarros  
PID: 100-000779-00  
I.N. 202202090022773  
0.69 Ac. (D)
- 2 Cousin Jerry LLC  
PID: 100-007055-00  
I.N. 202505150050528  
Original 2.323 Ac. (D)
- 3 Cousin Jerry LLC  
PID: 100-007054-00  
I.N. 202505150050528  
Original 2.323 Ac. (D)
- 4 OHIO RAILWAY MUSEUM, INC.  
PID: 100-000529-00  
O.R. 29706 Pg. E09  
PARCEL SIX  
2.009 Ac. (D)
- 5 OHIO RAILWAY MUSEUM, INC.  
PID: 100-001239-00  
O.R. 29706 Pg. E09  
PARCEL SIX  
2.009 Ac. (D)
- 6 OHIO RAILWAY MUSEUM, INC.  
PID: 100-001239-00  
O.R. 29706 Pg. E09  
PARCEL FIVE  
1.716 Ac. (D)
- 7 Michael R. Payne  
PID: 100-001429-00  
I.N. 201507020089523
- 8 KAREN S. MILLER AND SHAWN E. MILLER  
PID: 100-006074-00  
I.N. 200103090048575  
PARCEL TWO  
0.0688 Ac. (D)
- 9 KAREN S. MILLER AND SHAWN E. MILLER  
PID: 100-002108-00  
I.N. 200103090048575  
PARCEL ONE
- 10 Martin R. Shumway  
PID: 100-001478-00  
I.N. 201607210093788
- 11 Martin R. Shumway  
PID: 100-001478-00  
I.N. 201607210093788
- 12 The Board of County Commissioners of Franklin County, Ohio  
D.B. 1688 Pg. 284  
0.402 Ac. (D)

Boundless Strategic Resources, Inc.  
PID: 100-000398-00  
I.N. 201402270024366  
I.N. 201910080133085 (Affidavit)  
I.N. 202505050045860 (Affidavit)  
Tract 1  
Original 40.934 Ac. (D)

Boundless Strategic Resources, Inc.  
PID: 100-001234-00  
I.N. 201402270024366  
I.N. 201910080133085 (Affidavit)  
I.N. 202505050045860 (Affidavit)  
Tract 1  
Original 40.934 Ac. (D)

Boundless Strategic Resources, Inc.  
PID: 100-00398-00  
I.N. 201402270024366  
I.N. 201910080133085 (Affidavit)  
I.N. 202505050045860 (Affidavit)  
Tract 3  
Original 40.934 Ac. (D)

Boundless Strategic Resources, Inc.  
PID: 100-00398-00  
I.N. 201402270024366  
I.N. 201910080133085 (Affidavit)  
I.N. 202505050045860 (Affidavit)  
Tract 1  
Original 40.934 Ac. (D)

POC  
20.0± Ac.

POB  
20.0± Ac.

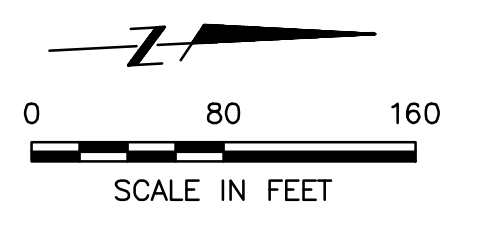
SUB-AREA 1  
20.0± Ac.

SUB-AREA 2  
0.4± Ac.

- LEGEND**
- R/W — EX. RIGHT OF WAY
  - PL — EX. PROPERTY LINE
  - PL — SUBJECT PROPERTY LINE
  - PR. PL — PR. PROPERTY LINE
  - — SUB-AREA BOUNDARY



CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026



REVISIONS	BY	DATE

**E. P. FERRIS**  
AND  
**ASSOCIATES**  
Consulting Civil Engineers and Surveyors

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(614) 299-2992 (Fax)  
www.EPFERRIS.com

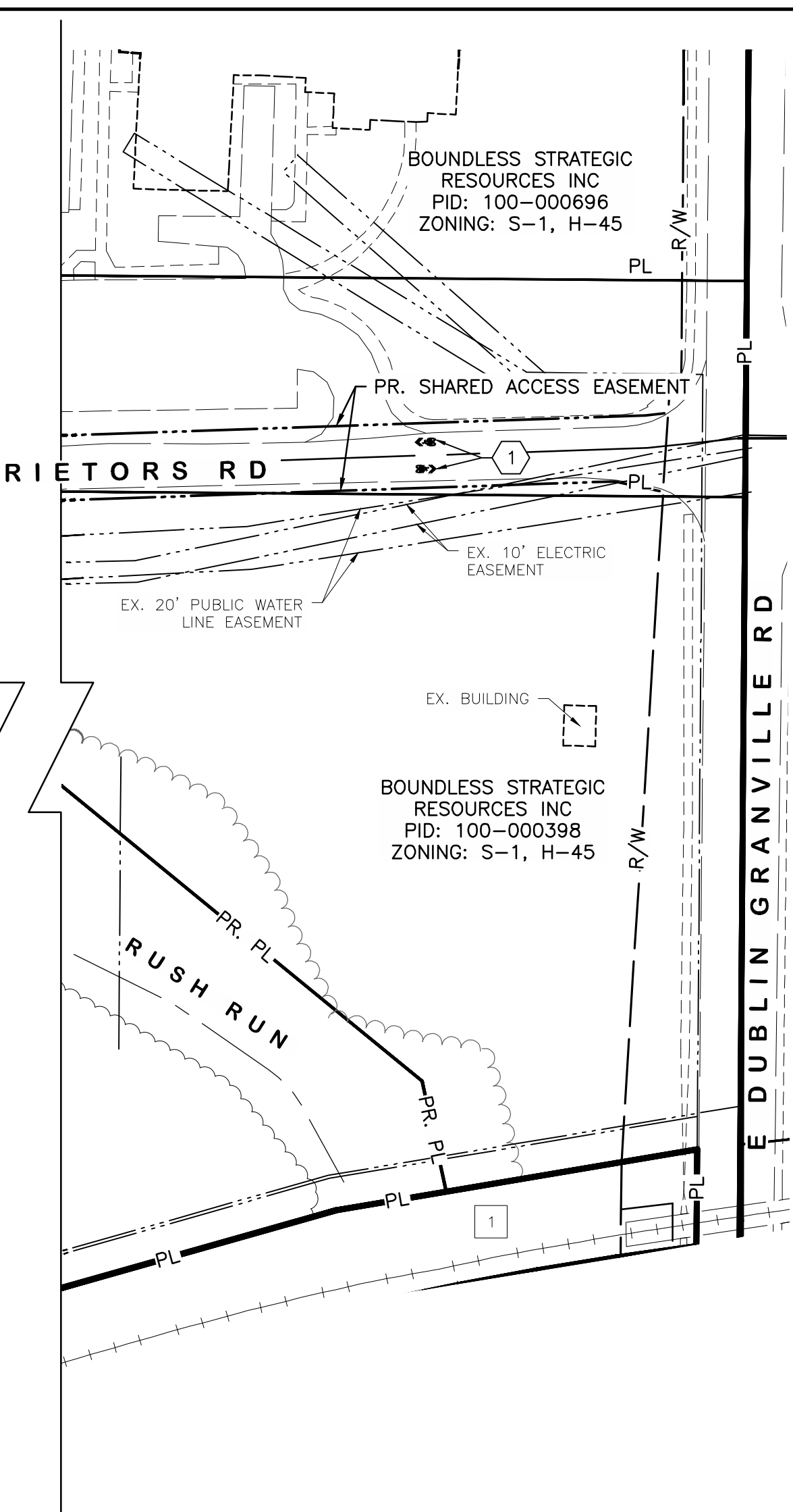
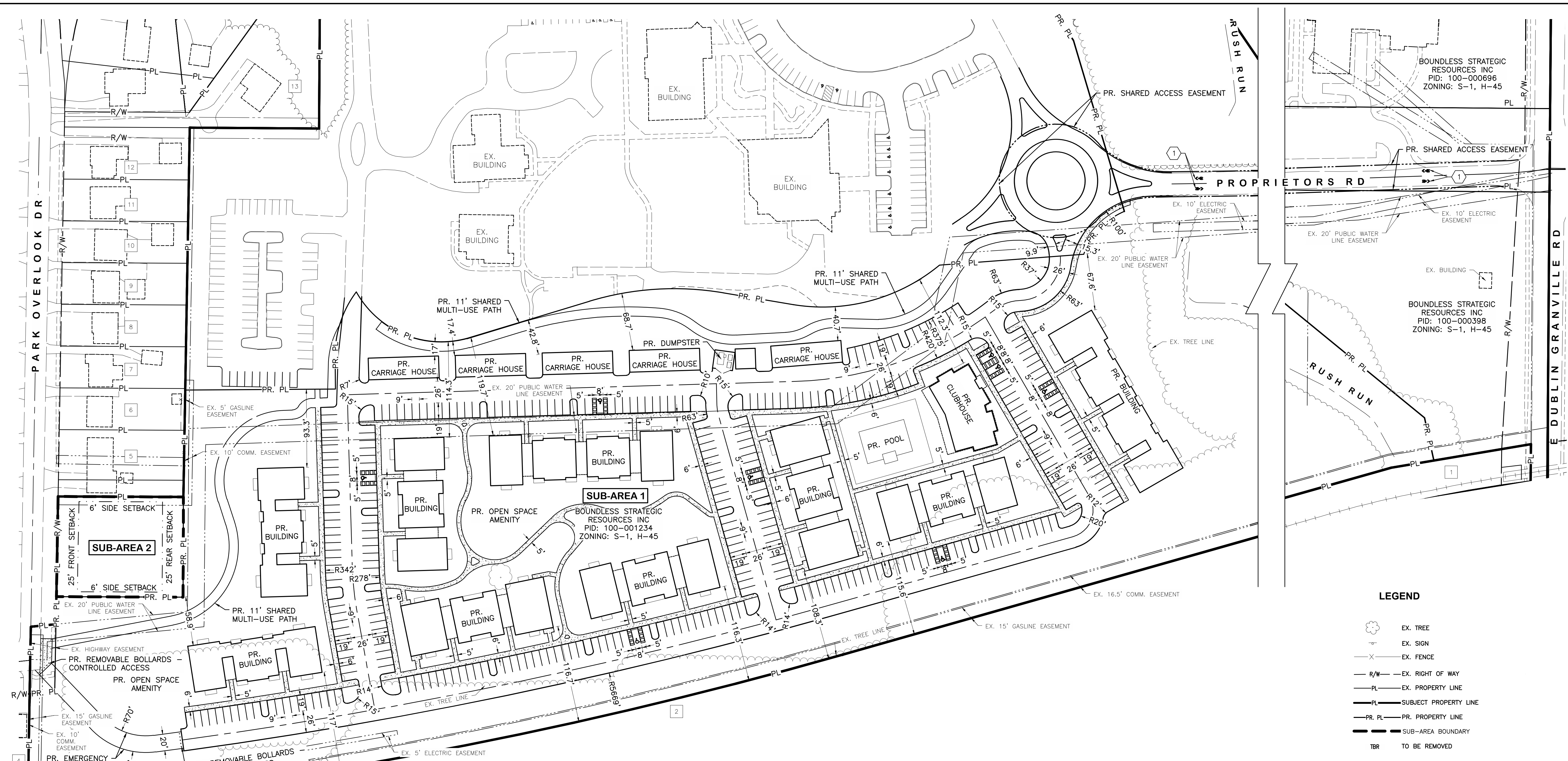
CITY OF WORTHINGTON, OHIO  
**BOUNDLESS**  
ELFORD DEVELOPMENT

JOB NO.:	1050.030
DESIGNED BY:	SKD
DRAWN BY:	SKD
CHECKED BY:	JAR
APPROVED BY:	CLP
DATE:	5/15/2026

**PRELIMINARY ZONING PLAT**

SCALE:	1" = 80'		
SHEET NO.	3	OF	6

M:\1050030\_Boundless\DWG\Production Drawings\1050030\_Preliminary\Site Layout.dwg --Site Layout Plan PLOTTED BY JAR ON 05/14/2026 11:20



- LEGEND**
- EX. TREE
  - EX. SIGN
  - EX. FENCE
  - R/W EX. RIGHT OF WAY
  - PL EX. PROPERTY LINE
  - PL SUBJECT PROPERTY LINE
  - PR. PL PR. PROPERTY LINE
  - SUB-AREA BOUNDARY
  - TBR TO BE REMOVED
  - PR. CONCRETE WALK/DRIVE

**NOTES:**  
ALL RADII ARE 4' UNLESS OTHERWISE NOTED. ALL RADIUS DIMENSIONS GIVEN ARE MEASURED ALONG FACE OF CURB.

**PHASING NOTE:**  
THERE WILL NOT BE ANY PHASES FOR THIS PROJECT AS EVERYTHING WILL BE BUILT AT ONCE.

- |   |  |
|---|--|
| 1 OHIO RAILWAY MUSEUM INC<br>PID: 100-000529                | 8 WARREN MARK D<br>PID: 100-001399<br>ZONING: R-10, H-40         |
| 2 OHIO RAILWAY MUSEUM INC<br>PID: 100-001239                | 9 BARTELT WILLIAM F III<br>PID: 100-001485<br>ZONING: R-10, H-40 |
| 3 OHIO RAILWAY MUSEUM INC<br>PID: 100-001998                | 10 DEWINE AMANDA<br>PID: 100-001476<br>ZONING: R-10, H-40        |
| 4 MILLER KAREN S<br>PID: 100-006074<br>ZONING: R-10, H-40   | 11 SUTHERLAND LAURIE A<br>PID: 100-001477<br>ZONING: R-10, H-40  |
| 5 MILLER KAREN S<br>PID: 100-002108<br>ZONING: R-10, H-40   | 12 SMULLEN JAKE<br>PID: 100-001480<br>ZONING: R-10, H-40         |
| 6 SHUMWAY MARTIN R<br>PID: 100-001478<br>ZONING: R-10, H-40 | 13 HOSTER BARRET B<br>PID: 100-001348<br>ZONING: R-10, H-40      |
| 7 SHUMWAY MARTIN R<br>PID: 100-001479<br>ZONING: R-10, H-40 |  |

**CODED NOTES**

① PR. "SHARROW" PAVEMENT MARKING

REVISIONS	BY	DATE

**E. P. FERRIS**  
AND  
**ASSOCIATES**  
Consulting Civil Engineers and Surveyors

2130 QUARRY TRAILS DR,  
2ND FLOOR  
COLUMBUS, OHIO 43228  
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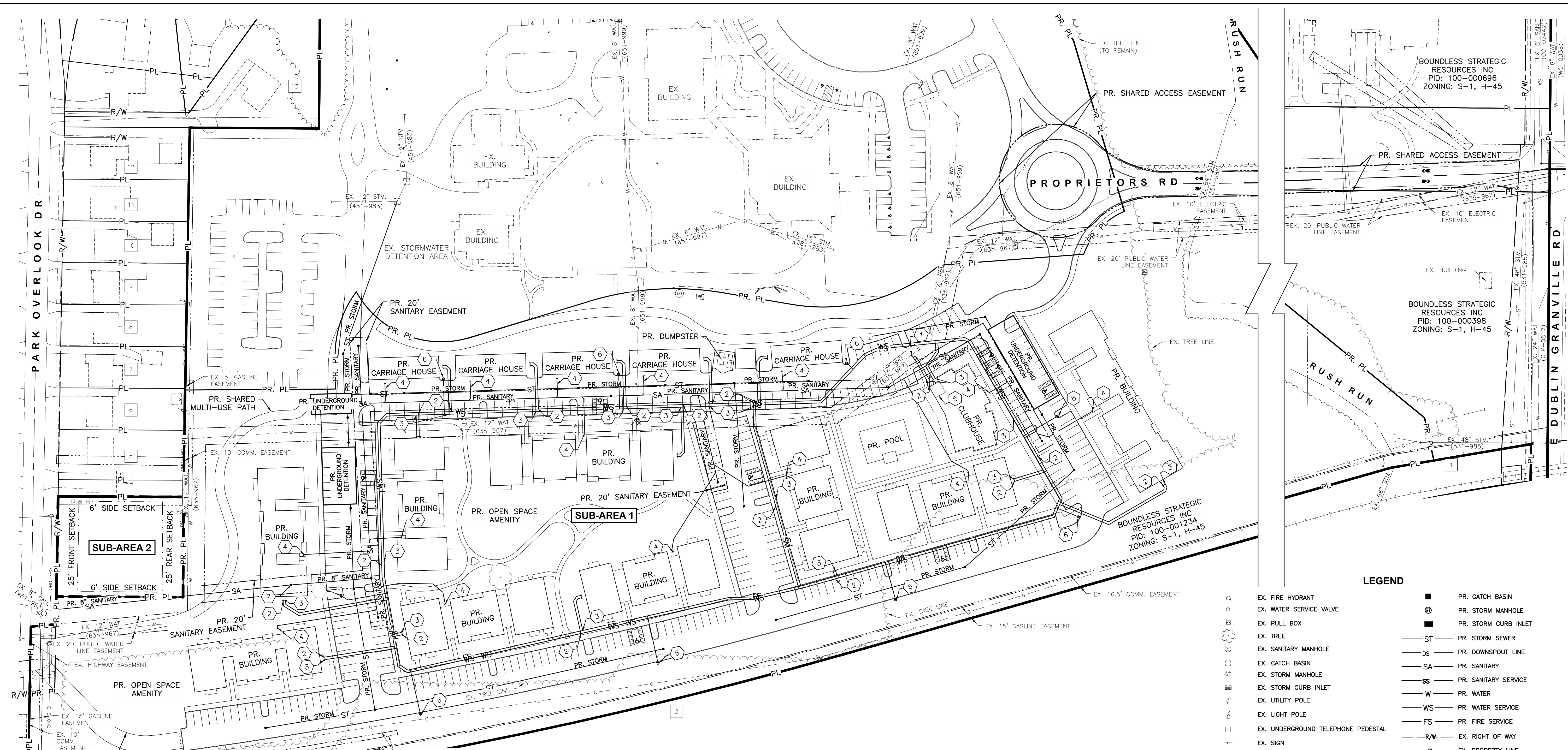
CITY OF WORTHINGTON, OHIO  
**BOUNDLESS**  
ELFORD DEVELOPMENT

JOB NO.:	1050.030
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CHECKED BY:	JAR
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DATE:	5/15/2026

**SITE LAYOUT PLAN**

SCALE: 1" = 60'	
SHEET NO. 4	OF 6

W:\1050030\_Boundless\DWG\Production Drawings\1050030\_Preliminary\Utility Plan.dwg --Site Utility Plan PLOTTED BY:RAY ON 05/14/2026 11:20



- LEGEND**
- ⊙ EX. FIRE HYDRANT
  - ⊕ EX. WATER SERVICE VALVE
  - ⊞ EX. PULL BOX
  - ⊗ EX. TREE
  - ⊙ EX. SANITARY MANHOLE
  - ⊙ EX. CATCH BASIN
  - ⊙ EX. STORM MANHOLE
  - ⊙ EX. STORM CURB INLET
  - ⊙ EX. UTILITY POLE
  - ⊙ EX. LIGHT POLE
  - ⊙ EX. UNDERGROUND TELEPHONE PEDESTAL
  - ⊙ EX. SIGN
  - ⊙ EX. FENCE
  - EX. WATER LINE
  - WS — EX. WATER SERVICE
  - UGT — EX. UNDERGROUND TELEPHONE
  - G — EX. GAS
  - ST — EX. STORM
  - SA — EX. SANITARY
  - USE — EX. UNDERGROUND ELECTRIC
  - OHE — EX. OVERHEAD ELECTRIC
  - PR. CATCH BASIN
  - ⊙ PR. STORM MANHOLE
  - ⊞ PR. STORM CURB INLET
  - ST — PR. STORM SEWER
  - DS — PR. DOWNSPOUT LINE
  - SA — PR. SANITARY
  - SS — PR. SANITARY SERVICE
  - W — PR. WATER
  - WS — PR. WATER SERVICE
  - FS — PR. FIRE SERVICE
  - R/W — EX. RIGHT OF WAY
  - PL — EX. PROPERTY LINE
  - — SUBJECT PROPERTY LINE
  - — SUB-AREA BOUNDARY
  - PR. PL — PR. PROPERTY LINE
  - TBR TO BE REMOVED
- CODED NOTES**
- ① PR. WATER TAP
  - ② PR. DOMESTIC WATER SERVICE
  - ③ PR. FIRE WATER SERVICE
  - ④ PR. SANITARY LATERAL
  - ⑤ PR. WATER METER ROOM
  - ⑥ PR. FIRE HYDRANT
  - ⑦ EX. INVERT +/- -9' FROM EX. GROUND

- 1 OHIO RAILWAY MUSEUM INC  
PID: 100-000529
- 2 OHIO RAILWAY MUSEUM INC  
PID: 100-001239
- 3 OHIO RAILWAY MUSEUM INC  
PID: 100-001998
- 4 MILLER KAREN S  
PID: 100-006074  
ZONING: R-10, H-40
- 5 MILLER KAREN S  
PID: 100-002108  
ZONING: R-10, H-40
- 6 SHUMWAY MARTIN R  
PID: 100-001478  
ZONING: R-10, H-40
- 7 SHUMWAY MARTIN R  
PID: 100-001479  
ZONING: R-10, H-40
- 8 WARREN MARK D  
PID: 100-001399  
ZONING: R-10, H-40
- 9 BARTELT WILLIAM F III  
PID: 100-001485  
ZONING: R-10, H-40
- 10 DEWINE AMANDA  
PID: 100-001476  
ZONING: R-10, H-40
- 11 SUTHERLAND LAURIE A  
PID: 100-001477  
ZONING: R-10, H-40
- 12 SMULLEN JAKE  
PID: 100-001480  
ZONING: R-10, H-40
- 13 HOSTER BARRET B  
PID: 100-001348  
ZONING: R-10, H-40

REVISIONS	BY	DATE

**E. P. FERRIS**  
AND  
**ASSOCIATES**  
Consulting Civil Engineers and Surveyors

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CITY OF WORTHINGTON, OHIO  
**BOUNDLESS**  
ELFORD DEVELOPMENT

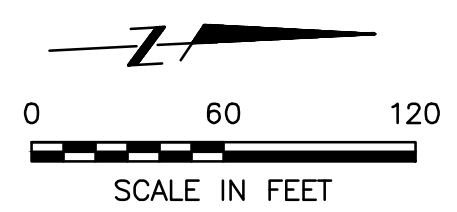
JOB NO.:	1050.030
DESIGNED BY:	SKD
DRAWN BY:	SKD
CHECKED BY:	JAR
APPROVED BY:	CLP
DATE:	5/15/2026

**SITE UTILITY PLAN**

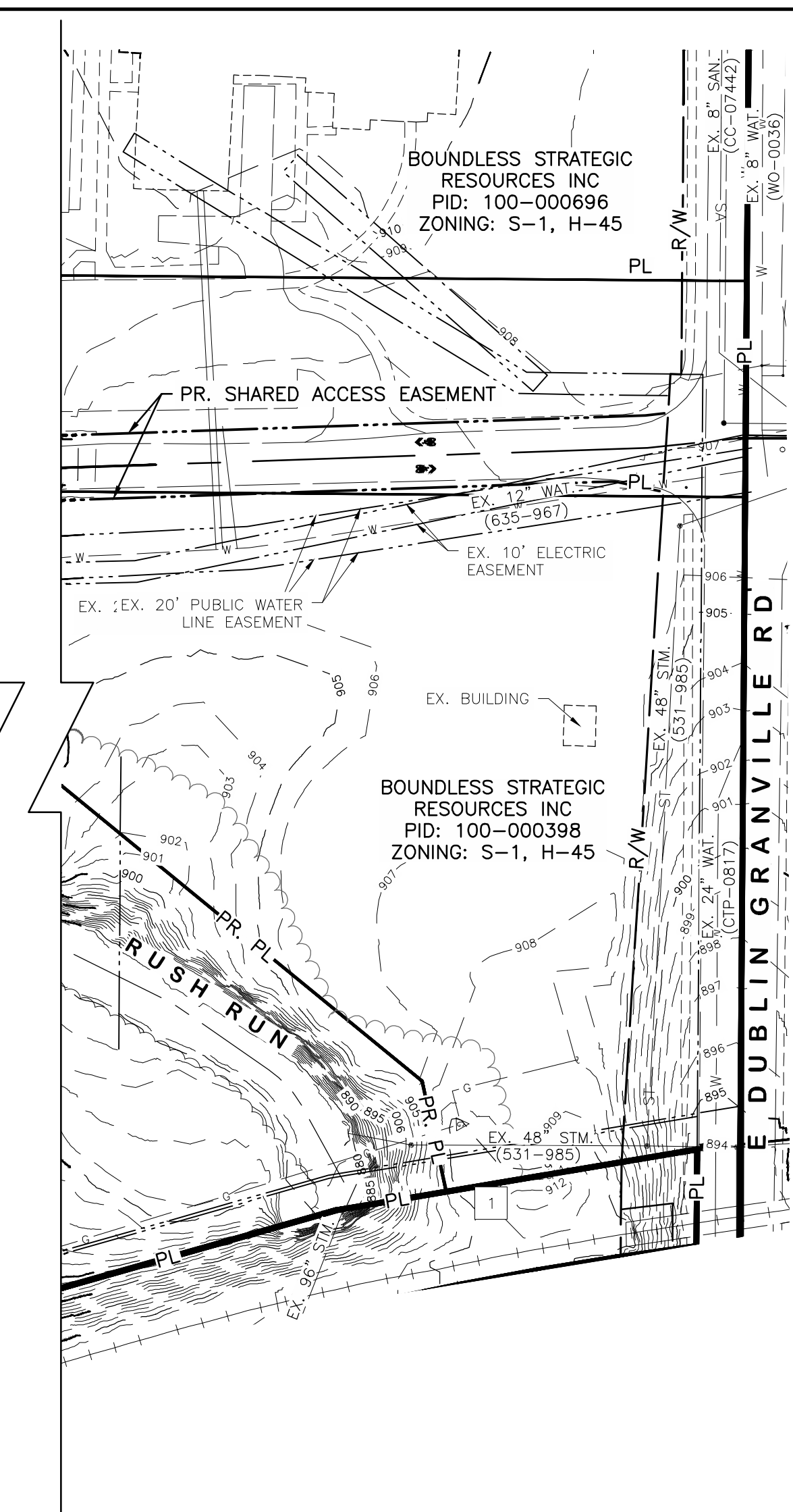
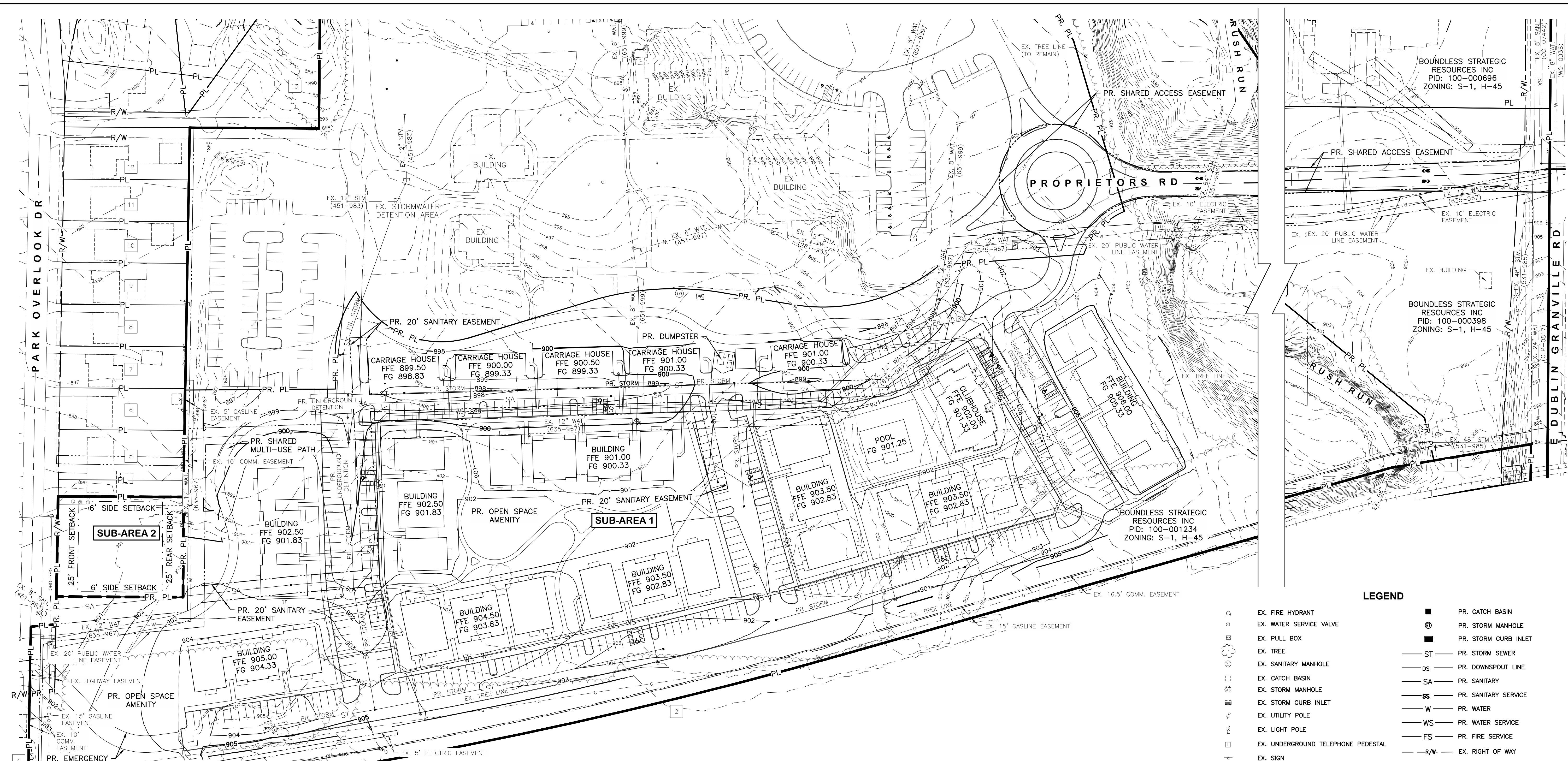
SCALE:	1" = 60'		
SHEET NO.	5	OF	6



CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026



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**LEGEND**

	EX. FIRE HYDRANT		PR. CATCH BASIN
	EX. WATER SERVICE VALVE		PR. STORM MANHOLE
	EX. PULL BOX		PR. STORM CURB INLET
	EX. TREE		PR. STORM SEWER
	EX. SANITARY MANHOLE		PR. DOWNSPOUT LINE
	EX. CATCH BASIN		PR. SANITARY
	EX. STORM MANHOLE		PR. STORM SERVICE
	EX. STORM CURB INLET		PR. WATER
	EX. UTILITY POLE		PR. WATER SERVICE
	EX. LIGHT POLE		PR. FIRE SERVICE
	EX. UNDERGROUND TELEPHONE PEDESTAL		R/W EX. RIGHT OF WAY
	EX. SIGN		PL EX. PROPERTY LINE
	EX. FENCE		PL SUBJECT PROPERTY LINE
	EX. WATER LINE		PR. PL PR. PROPERTY LINE
	EX. WATER SERVICE		-996 EX. MINOR CONTOUR ELEVATION
	EX. UNDERGROUND TELEPHONE		-995 EX. MAJOR CONTOUR ELEVATION
	EX. GAS		-996 EX. MINOR CONTOUR ELEVATION
	EX. STORM		-995 EX. MAJOR CONTOUR ELEVATION
	EX. SANITARY		--- SUB-AREA BOUNDARY
	EX. UNDERGROUND ELECTRIC		TBR TO BE REMOVED
	EX. OVERHEAD ELECTRIC		

- |   |  |
|---|--|
| 1 OHIO RAILWAY MUSEUM INC<br>PID: 100-000529                | 8 WARREN MARK D<br>PID: 100-001399<br>ZONING: R-10, H-40         |
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| 3 OHIO RAILWAY MUSEUM INC<br>PID: 100-001998                | 10 DEWINE AMANDA<br>PID: 100-001476<br>ZONING: R-10, H-40        |
| 4 MILLER KAREN S<br>PID: 100-006074<br>ZONING: R-10, H-40   | 11 SUTHERLAND LAURIE A<br>PID: 100-001477<br>ZONING: R-10, H-40  |
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CITY OF WORTHINGTON  
 DRAWING NO. PUD 01-2026  
 DATE 06/17/2026

0 60 120  
 SCALE IN FEET

REVISIONS	BY	DATE

**E. P. FERRIS**  
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**ASSOCIATES**  
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CITY OF WORTHINGTON, OHIO  
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APPROVED BY:	CLP
DATE:	5/15/2026

<b>PRELIMINARY GRADING PLAN</b>		SCALE:	1" = 60'
		SHEET NO.	6 OF 6



**1** Tree Preservation Plan  
SCALE: 1" = 80'-0"

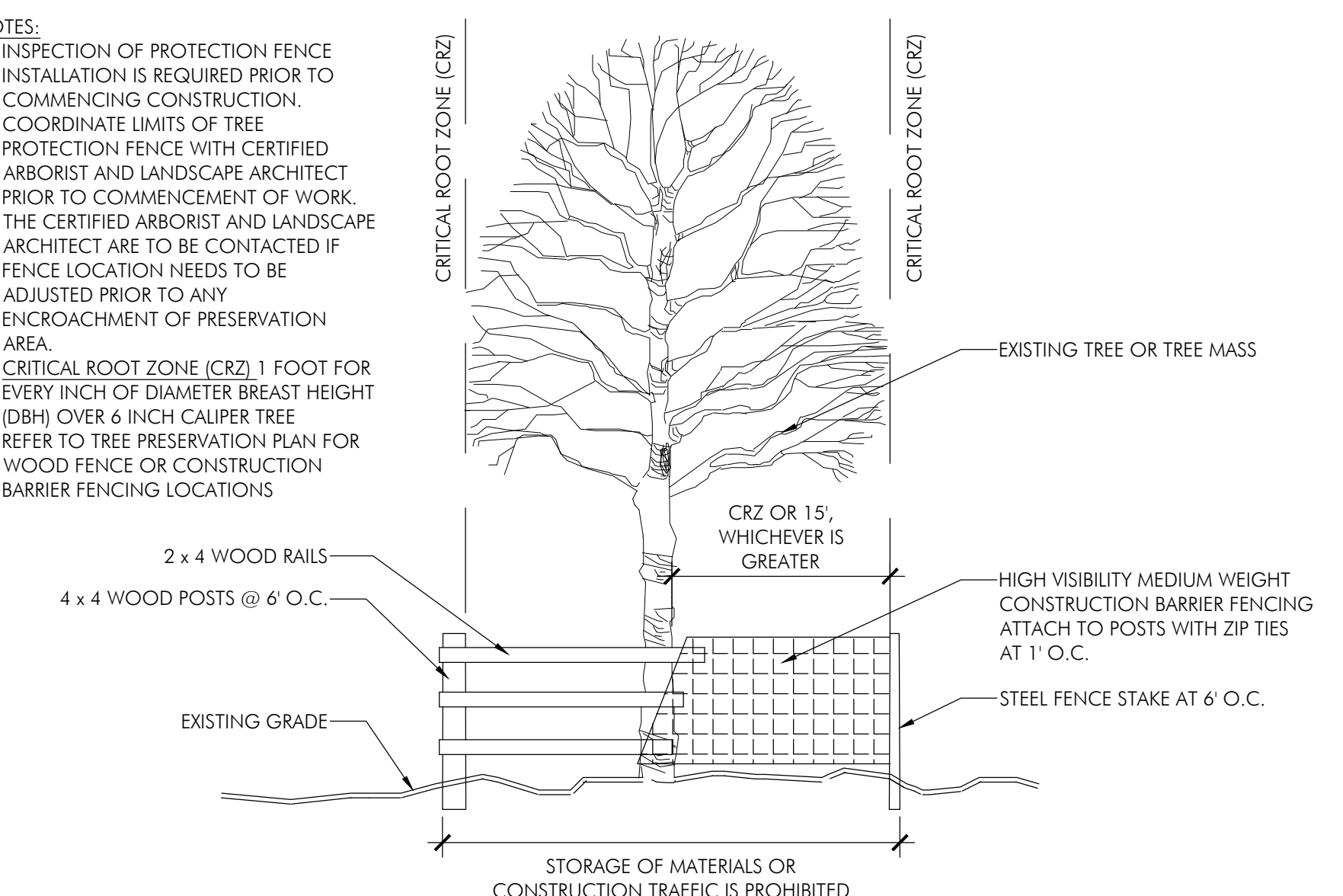
**LEGEND**

- PROPERTY LINE
- EXISTING TREE LINE TO REMAIN
- EXISTING TREE TO REMAIN AND BE PROTECTED, SEE DETAIL, THIS SHEET
- EXISTING TREE TO BE REMOVED, SEE SHEET L1.1 FOR SPECIES INFORMATION & REMOVAL CALCULATIONS

NOTE: TREE REMOVAL AND PRESERVATION SUBJECT TO CHANGE PER FINAL ENGINEERING AND COORDINATION WITH CITY STAFF.

**NOTES:**

1. INSPECTION OF PROTECTION FENCE INSTALLATION IS REQUIRED PRIOR TO COMMENCING CONSTRUCTION.
2. COORDINATE LIMITS OF TREE PROTECTION FENCE WITH CERTIFIED ARBORIST AND LANDSCAPE ARCHITECT PRIOR TO COMMENCEMENT OF WORK.
3. THE CERTIFIED ARBORIST AND LANDSCAPE ARCHITECT ARE TO BE CONTACTED IF FENCE LOCATION NEEDS TO BE ADJUSTED PRIOR TO ANY ENCROACHMENT OF PRESERVATION AREA.
4. CRITICAL ROOT ZONE (CRZ) 1 FOOT FOR EVERY INCH OF DIAMETER BREAST HEIGHT (DBH) OVER 6 INCH CALIPER TREE
5. REFER TO TREE PRESERVATION PLAN FOR WOOD FENCE OR CONSTRUCTION BARRIER FENCING LOCATIONS



**2** Tree Preservation Detail



**Columbus**  
100 Northwoods Blvd, Ste A  
Columbus, Ohio 43235  
p 614.255.3399

**Cincinnati**  
20 Village Square, Floor 3  
Cincinnati, Ohio 45246  
p 614.360.3066

PODdesign.net

**Project Name**

**Elford Boundless**

445 E Dublin Granville Rd.  
Worthington, OH 43085



**Prepared For**

Elford Development  
1220 Dublin Rd  
Columbus, OH 43215

**Project Info**

Project # 25059  
Date 05/15/2026  
By ADC, ANC  
Scale As Shown

**Revisions**

**Sheet Title**

**TREE PRESERVATION PLAN**



**Sheet #**

**L1.0**



CITY OF WORTHINGTON  
DRAWING NO. PUD 01-2026  
DATE 06/17/2026

TREE ID	DBH	TREE TYPE	TREE CONDITION	SATUS
1	18-24	SPRUCE (NORWAY)	FAIR	REMOVE
2	18-24	SPRUCE (NORWAY)	FAIR	REMOVE
4	24-30	OAK (PIN)	FAIR	
5	18-24	OAK (PIN)	FAIR	REMOVE
6	18-24	OAK (PIN)	POOR	REMOVE
7	12-18	OAK (WHITE)	VERY POOR	REMOVE
17	12-18	PINE (WHITE)	VERY POOR	REMOVE
19	24-30	LOCUST (HONEY)	VERY POOR	REMOVE
20	18-24	LOCUST (HONEY)	VERY POOR	REMOVE
22	18-24	LOCUST (HONEY)	VERY POOR	REMOVE
32	24-30	RED MAPLE	VERY POOR	REMOVE
42	12-18	OAK (RED)	POOR	REMOVE
51	18-24	OAK (PIN)	FAIR	
52	12-18	OAK (BUR)	FAIR	
53	18-24	LOCUST (HONEY)	POOR	REMOVE
54	18-24	LOCUST (HONEY)	POOR	REMOVE
76	18-24	RED MAPLE	FAIR	REMOVE
77	36-42	SWEETGUM	GOOD	REMOVE
78	42+	OAK (PIN)	FAIR	REMOVE
79	18-24	RED MAPLE	FAIR	REMOVE
80	42+	BASSWOOD (AMERICAN)	FAIR	REMOVE
81	24-30	MAPLE (SUGAR)	FAIR	
82	42+	OAK (SWAMP WHITE)	POOR	REMOVE
83	42+	OAK (SWAMP WHITE)	POOR	REMOVE
84	42+	OAK (SWAMP WHITE)	POOR	REMOVE
85	42+	OAK (SWAMP WHITE)	POOR	REMOVE
86	30-36	OAK (SWAMP WHITE)	POOR	REMOVE
87	30-36	OAK (SWAMP WHITE)	POOR	REMOVE
88	30-36	OAK (SWAMP WHITE)	POOR	REMOVE
89	42+	OAK (SWAMP WHITE)	POOR	REMOVE
90	36-42	OAK (SWAMP WHITE)	POOR	REMOVE
91	36-42	OAK (SWAMP WHITE)	POOR	REMOVE
92	36-42	OAK (SWAMP WHITE)	POOR	REMOVE
93	42+	OAK (SWAMP WHITE)	POOR	REMOVE
94	30-36	OAK (SWAMP WHITE)	POOR	REMOVE
95	30-36	OAK (SWAMP WHITE)	POOR	REMOVE
96	18-24	HICKORY (SHAGBARK)	GOOD	REMOVE
117	12-18	OAK (PIN)	FAIR	REMOVE
118	12-18	OAK (PIN)	FAIR	REMOVE
119	42+	OAK (SWAMP WHITE)	FAIR	
120	24-30	OAK (SWAMP WHITE)	FAIR	
121	12-18	OAK (SWAMP WHITE)	FAIR	REMOVE
122	24-30	HICKORY (SHAGBARK)	FAIR	REMOVE
123	24-30	SPRUCE (NORWAY)	FAIR	REMOVE
124	30-36	SWEETGUM	POOR	REMOVE
125	12-18	MAPLE (SUGAR)	DEAD	REMOVE
126	12-18	MAPLE (SUGAR)	DEAD	REMOVE
127	12-18	MAPLE (SUGAR)	DEAD	REMOVE
128	12-18	MAPLE (SUGAR)	DEAD	REMOVE
129	12-18	NORWAY MAPLE	VERY POOR	REMOVE
130	12-18	NORWAY MAPLE	VERY POOR	REMOVE
131	12-18	NORWAY MAPLE	VERY POOR	REMOVE
132	18-24	FRAXINUS SP.	POOR	REMOVE
133	12-18	CRATAEGUS SP.	DEAD	REMOVE
162	18-24	SPRUCE (NORWAY)	FAIR	REMOVE
163	18-24	SPRUCE (NORWAY)	FAIR	REMOVE
164	18-24	SPRUCE (NORWAY)	FAIR	REMOVE
165	24-30	SPRUCE (NORWAY)	FAIR	REMOVE
166	12-18	SPRUCE (NORWAY)	FAIR	REMOVE
167	18-24	SPRUCE (NORWAY)	FAIR	REMOVE

168	18-24	MAPLE (SUGAR)	POOR	REMOVE
169	24-30	OAK (RED)	FAIR	REMOVE
170	18-24	FRAXINUS SP.	POOR	REMOVE
171	18-24	OAK (RED)	FAIR	REMOVE
172	12-18	OAK (PIN)	GOOD	REMOVE
173	24-30	SPRUCE SP.	FAIR	REMOVE
174	36-42	TULIP TREE	FAIR	REMOVE
175	30-36	WHITE MULBERRY	POOR	REMOVE
176	30-36	NORWAY MAPLE	VERY POOR	REMOVE
177	30-36	BASSWOOD (AMERICAN)	FAIR	REMOVE
178	30-36	BALDCYPRESS	GOOD	REMOVE
179	30-36	NORWAY MAPLE	VERY POOR	REMOVE
180	30-36	NORWAY MAPLE	POOR	REMOVE
181	12-18	REDBUD (EASTERN)	POOR	REMOVE
182	12-18	WALNUT (BLACK)	FAIR	REMOVE
183	18-24	WALNUT (BLACK)	FAIR	REMOVE
184	18-24	WALNUT (BLACK)	FAIR	REMOVE
185	12-18	SPRUCE (BLUE)	DEAD	REMOVE
186	12-18	SPRUCE (BLUE)	VERY POOR	REMOVE
187	30-36	BALDCYPRESS	GOOD	REMOVE
188	12-18	MAPLE (SUGAR)	FAIR	REMOVE
189	12-18	RED MAPLE	POOR	REMOVE
190	12-18	OAK (PIN)	VERY POOR	REMOVE
191	18-24	SPRUCE (NORWAY)	FAIR	
192	12-18	MAPLE (SUGAR)	FAIR	REMOVE
193	18-24	OAK (PIN)	FAIR	
194	30-36	OAK (PIN)	FAIR	
195	30-36	OAK (PIN)	POOR	REMOVE
197	24-30	MAPLE (SUGAR)	FAIR	REMOVE
198	30-36	MAPLE (SUGAR)	FAIR	
199	30-36	MAPLE (SUGAR)	POOR	REMOVE
200	24-30	BALDCYPRESS	GOOD	REMOVE
201	24-30	BALDCYPRESS	GOOD	REMOVE
202	24-30	OAK (PIN)	FAIR	REMOVE
203	24-30	OAK (PIN)	POOR	REMOVE
204	12-18	RED MAPLE	POOR	REMOVE
205	12-18	SWEETGUM	FAIR	REMOVE
206	30-36	BALDCYPRESS	GOOD	
210	24-30	LOCUST (HONEY)	POOR	REMOVE
211	12-18	LOCUST (HONEY)	POOR	REMOVE
212	24-30	LOCUST (HONEY)	POOR	REMOVE
213	30-36	WALNUT (BLACK)	POOR	REMOVE
214	18-24	HACKBERRY	POOR	REMOVE
215	12-18	OAK (SWAMP WHITE)	FAIR	REMOVE
216	18-24	HACKBERRY	POOR	REMOVE
217	12-18	OAK (PIN)	GOOD	
218	12-18	OAK (PIN)	GOOD	
219	18-24	PEAR (BRADFORD)	VERY POOR	REMOVE
220	18-24	SWEETGUM	POOR	REMOVE
221	18-24	PEAR (BRADFORD)	VERY POOR	REMOVE
222	12-18	HACKBERRY	POOR	REMOVE
223	12-18	RED MAPLE	VERY POOR	REMOVE
224	12-18	RED MAPLE	POOR	REMOVE
226	36-42	OAK (PIN)	FAIR	REMOVE
227	24-30	SHUMARD OAK	FAIR	REMOVE
228	12-18	OAK (WHITE)	POOR	REMOVE
234	12-18	SPRUCE (NORWAY)	DEAD	REMOVE
269	18-24	FRAXINUS SP.	VERY POOR	REMOVE
270	18-24	WHITE PINE	FAIR	
272	12-18	N/A	DEAD	REMOVE
275	12-18	N/A	DEAD	REMOVE
276	12-18	N/A	DEAD	REMOVE
277	12-18	N/A	DEAD	REMOVE
278	12-18	N/A	DEAD	REMOVE

279	12-18	N/A	DEAD	REMOVE
281	18-24	N/A	VERY POOR	REMOVE
289	12-18	WHITE OAK	GOOD	REMOVE
290	12-18	SCOTCH PINE	FAIR	
291	12-18	OAK (SWAMP WHITE)	FAIR	
292	12-18	OAK (SWAMP WHITE)	FAIR	
293	12-18	BLUE SPRUCE	VERY POOR	REMOVE
294	12-18	OHIO BUCKEYE	FAIR	
295	12-18	GINKGO	POOR	REMOVE
296	12-18	SHINGLE OAK	FAIR	
330	18-24	BALD CYPRESS	GOOD	
331	18-24	PIGNET HICKORY	FAIR	REMOVE
382	6-9	JAPANESE CRABAPPLE	VERY POOR	REMOVE
383	6-9	JAPANESE CRABAPPLE	VERY POOR	REMOVE
384	6-9	JAPANESE CRABAPPLE	VERY POOR	REMOVE
408	6-9	REDBUD (EASTERN)	FAIR	REMOVE
409	6-9	FRAXINUS SP.	VERY POOR	REMOVE
410	6-9	OAK (SWAMP WHITE)	FAIR	
411	6-9	MAPLE (SUGAR)	FAIR	REMOVE
413	6-9	HICKORY (SHAGBARK)	FAIR	REMOVE
414	6-9	HICKORY (SHAGBARK)	FAIR	
415	6-9	WALNUT (BLACK)	VERY POOR	REMOVE
416	6-9	COLORADO BLUE SPRUCE	VERY POOR	REMOVE
417	6-9	NORWAY MAPLE	FAIR	REMOVE
418	6-9	SWEETGUM	POOR	REMOVE
419	6-9	SWEETGUM	POOR	REMOVE
420	6-9	SWEETGUM	POOR	REMOVE
421	6-9	SWEETGUM	POOR	REMOVE
422	6-9	SWEETGUM	POOR	REMOVE
437	6-9	N/A	POOR	REMOVE
439	6-9	N/A	DEAD	REMOVE
447	6-9	N/A	DEAD	REMOVE
451	9-12	RED MAPLE	POOR	REMOVE
454	9-12	MAPLE (SUGAR)	GOOD	
455	30	LONDON PLANE	GOOD	
456	18-24	WHITE PINE	GOOD	
457	18-24	WHITE PINE	FAIR	
458	18-24	WHITE PINE	GOOD	
459	18-24	WHITE PINE	GOOD	

**TREES REMOVED NOTES**

2920 TOTAL INCHES TO BE REMOVED  
(135 TREES)

1853 CALIPER INCHES OF TREES WHICH ARE TO BE REMOVED DUE TO THE POOR, VERY POOR OR DEAD CONDITIONS  
(88 TREES)

1172 CALIPER INCHES OF TREES WHICH ARE TO BE REMOVED ARE IN FAIR TO GOOD CONDITION  
(47 TREES)

**TREE PRESERVATION, REMOVAL, & REPLACEMENT NOTES**

- ALL TREES IN THE CONSTRUCTION AREA NOT SPECIFICALLY DESIGNATED FOR REMOVAL SHALL BE PRESERVED. ALL TREE REMOVAL AND PRESERVATION WORK SHALL BE SUPERVISED WITH THE INPUT OF A CERTIFIED ARBORIST.
- FENCING SHALL BE INSTALLED AROUND ALL TREES TO BE PRESERVED PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. THE FENCING MUST REMAIN IN PLACE DURING ALL PHASES OF CONSTRUCTION. ANY CHANGES IN THE PROTECTIVE FENCING MUST BE APPROVED BY THE LANDSCAPE INSPECTOR. OWNER SHALL BE RESPONSIBLE FOR THE CONSTRUCTION, ERECTION, AND MAINTENANCE OF PROTECTION FENCING; REFER TO TREE PRESERVATION DETAIL.
- PRESERVED TREES REMAINING ARE TO BE FERTILIZED AND PRUNED (IF NEEDED) TWO YEARS AFTER CONSTRUCTION.
- OWNER SHALL MONITOR HEALTH AND VIGOR OF EXISTING TREES THROUGHOUT THE COURSE OF CONSTRUCTION OF THIS PROJECT.
- CONTRACTOR TO NOTIFY A CERTIFIED ARBORIST IF ANY DECLINE IN HEALTHY TREES IS APPARENT. THE ARBORIST SHALL DETERMINE THE REMEDIAL ACTION, IF ANY, TO BE TAKEN.
- ALL EXISTING TREES NOTED AS FAIR TO POOR HEALTH SHALL BE FERTILIZED YEARLY IN THE SPRING USING A HORTICULTURALLY APPROVED METHOD FOR A PERIOD OF THREE YEARS. AFTER THREE YEARS, THE HEALTH OF THE TREE SHOULD BE EVALUATED BY A CERTIFIED ARBORIST AND CONTINUING MAINTENANCE RECOMMENDATIONS SHALL BE MADE.
- ANY TREES REMOVED OR DAMAGED IN TREE PRESERVATION WILL HAVE TO BE REPLACED PER CODE.
- FAILURE TO ABIDE BY THESE TERMS OF THE TREE PRESERVATION, REMOVAL, AND REPLACEMENT SHALL BE UNLAWFUL.



**Columbus**  
100 Northwoods Blvd, Ste A  
Columbus, Ohio 43235  
p 614.255.3399

**Cincinnati**  
20 Village Square, Floor 3  
Cincinnati, Ohio 45246  
p 614.360.3066

PODdesign.net

**Project Name**

**Elford Boundless**

445 E Dublin Granville Rd.  
Worthington, OH 43085



**Prepared For**

Elford Development  
1220 Dublin Rd  
Columbus, OH 43215

**Project Info**

Project # 25059  
Date 05/15/2026  
By ADC, ANC  
Scale As Shown

**Revisions**

**Sheet Title**

**TREE SURVEY TABLE**

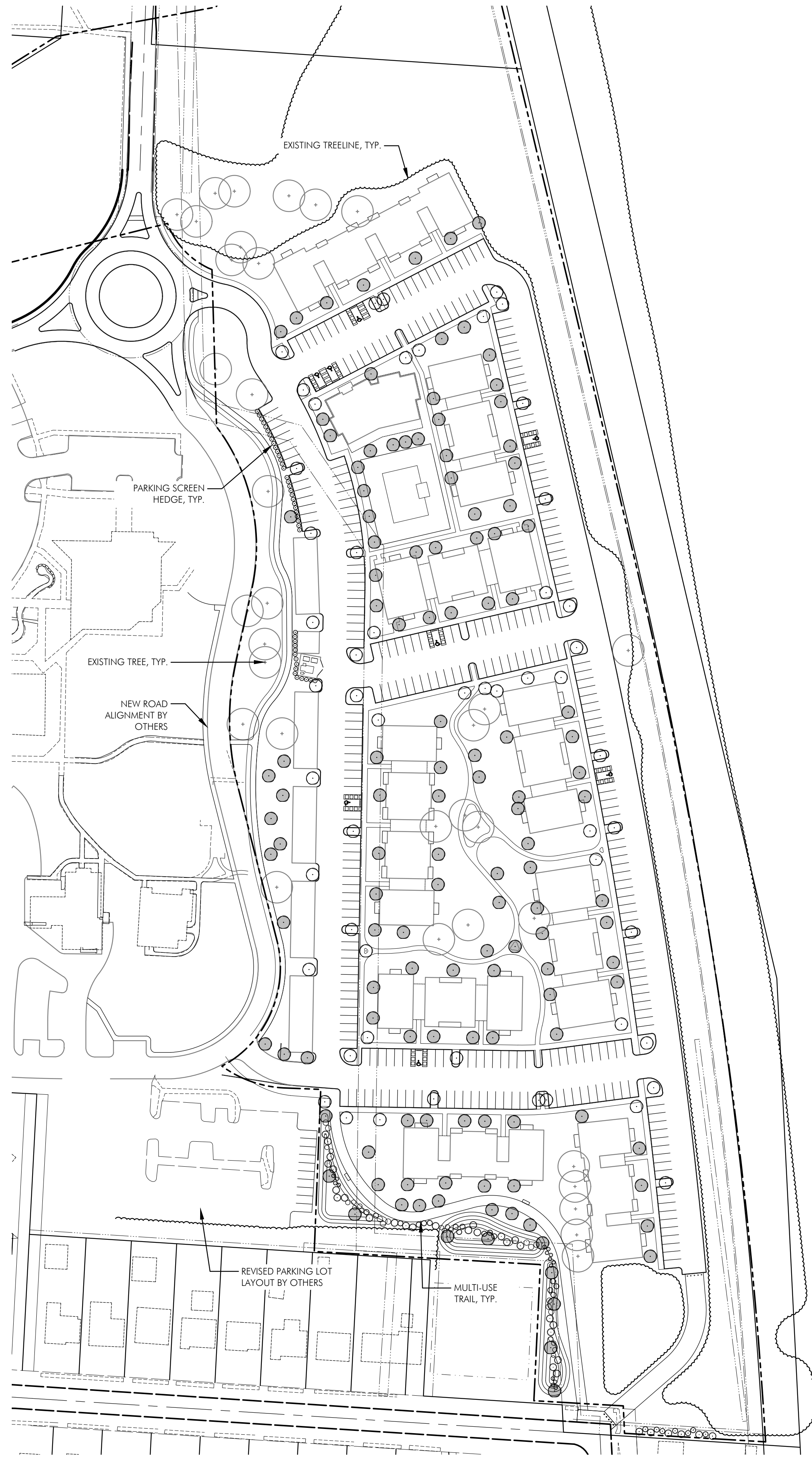


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DRAWING NO. PUD 01-2026  
DATE 06/17/2026

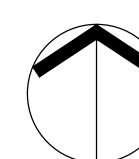


**Sheet #**

**L1.1**



**Tree Replacement Plan**  
SCALE: 1" = 80'-0"



**LEGEND**

- PROPERTY LINE
- EXISTING TREE LINE
- EXISTING TREE
- PROPOSED TREE (PER PARKING AREA CODE)
- PROPOSED TREE (PER TREE REPLACEMENT REQUIREMENTS)
- PROPOSED EVERGREEN TREE (FOR PERIMETER SCREENING)

**LANDSCAPE ZONING CODE REQUIREMENTS**

1171.02(h) LANDSCAPING OF PARKING AREAS  
 2" DBH TREE TRUNK FOR EVERY SIX PARKING SPACES  
 REQUIRED TREES: 54 (325 TOTAL PARKING SPACES/6)  
 PROPOSED TREES: 54

TOTAL TREES REMOVED: 1172 CALIPER INCHES / 47 TREES IN FAIR TO GOOD CONDITION  
 TOTAL TREES PROVIDED: ±688 CALIPER INCHES / 275 TREES (AVERAGE OF 2.5" CAL.)  
 54 PER PARKING LOT CODE REQUIREMENTS  
 91 FOR PERIMETER SCREENING

**CONCEPTUAL PLANT PALETTE**

SYMBOL	BOTANICAL & COMMON NAMES	SIZE	COND.
<b>DECIDUOUS TREES</b>			
AC SA	Acer saccharum 'Commemoration' Commemoration Sugar Maple	3" Cal.	B&B
AE GL	Aesculus glabra Ohio Buckeye	3" Cal.	B&B
TI TO	Tilia tomentosa 'Sterling' Silver Linden	2" Cal.	B&B
CE OC	Celtis occidentalis Hackberry	2" Cal.	B&B
GL TR	Gleditsia triacanthos inermis 'Skyline' Skyline Thornless Honeylocust	2.5" Cal.	B&B
GY DI	Gymnocladus dioica Kentucky Coffeetree	2.5" Cal.	B&B
PL AC	Platanus acerifolia 'Morton Circle' EXCLAMATION! Landon Planetree	3" Cal.	B&B
QU BI	Quercus bicolor Swamp White Oak	3" Cal.	B&B
QU MA	Quercus macrocarpa Bur Oak	3" Cal.	B&B
QU MO	Quercus montana Chestnut Oak	2.5" Cal.	B&B
QU PA	Quercus palustris Pin Oak	2" Cal.	B&B
QU RU	Quercus rubra Red Oak	3" Cal.	B&B
UL FR	Ulmus 'Frontier' Frontier Elm	2.5" Cal.	B&B
<b>EVERGREEN TREES</b>			
PI AB	Picea abies Norway Spruce	3" Cal.	B&B
TS CA	Tsuga canadensis Canadian Hemlock	3" Cal.	B&B
JU BU	Juniperus virginiana 'Burkii' Burkii Eastern Red Cedar	3" Cal.	B&B
PI ST	Pinus strobus White Pine	3" Cal.	B&B



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Columbus, OH 43215

**Project Info**

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Date 05/15/2026  
By ADC, ANC  
Scale As Shown

**Revisions**

**Sheet Title**

**TREE REPLACEMENT PLAN**



**Sheet #**

**L1.2**





**LEGEND**

- - - - - PROPERTY LINE
- (+) EXISTING TREE TO REMAIN

**GENERAL NOTES**

1. ALL AREAS OUTSIDE OF PAVING, BUILDING, AND PLANTING AREAS ARE TO BE SEEDED.
2. AREAS OF LAWN BETWEEN SIDEWALKS AND PLANT BEDS SHALL BE 24" MINIMUM WIDTH. WHERE LESS THAN 24" REMAINS, EXTEND PLANT BED TO SIDEWALK.

**PLANT LIST**

QTY	SYMBOL	BOTANICAL & COMMON NAMES	SIZE	CONDITION	REMARKS
<b>DECIDUOUS TREES</b>					
6	PL AC	Platanus x acerifolia 'Morton Circle' Exclamation! London Planetree	3" Cal.	B&B	Spacing As Shown
4	QU BI	Quercus bicolor Swamp White Oak	2.5" Cal.	B&B	Spacing As Shown
3	QU PA	Quercus palustris Pin Oak	2" cal.	B&B	Spacing As Shown
3	UL FR	Ulmus 'frontier' Frontier Elm	3" cal.	B&B	Spacing As Shown
<b>EVERGREEN TREES</b>					
12	PI AB	Picea abies Norway spruce	3" Cal.	B&B	Spacing As Shown
12	TS CA	Tsuga canadensis Canadian Hemlock	3" Cal.	B&B	Spacing As Shown
23	JU BU	Juniperus virginiana 'Burkii' Burkii Eastern Red Cedar	3" Cal.	B&B	Spacing As Shown
8	PI ST	Pinus strobus White Pine	3" Cal.	B&B	Spacing As Shown
<b>SHRUBS</b>					
11	FO IN	Forsythia x intermedia Forsythia	#3	Cont.	Spacing As Shown
15	JU CH	Juniperus chinensis 'Nick's Compact' Nick's Compact Juniper	#5	Cont.	Spacing As Shown
15	VI PL	Viburnum plicatum Snowball Viburnum	#3	Cont.	Spacing As Shown

\*PLANT SUBSTITUTIONS MAY BE REQUIRED PENDING ON PLANT AVAILABILITY. ANY SUBSTITUTIONS MUST BE APPROVED BY LANDSCAPE ARCHITECT AND SHALL MEET MINIMUM CODE REQUIREMENTS AT TIME OF INSTALLATION.

1 South Property Line Planting Plan  
Scale: 1" = 20'-0"



**Columbus**  
100 Northwoods Blvd, Ste A  
Columbus, Ohio 43235  
p 614.255.3399

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**Project Name**  
**Boundless Apartments**  
Worthington Ohio

**Prepared For**  
Elford Development  
1220 Dublin Road,  
Columbus, OH, 43215

PRELIMINARY  
NOT FOR CONSTRUCTION

**Project Info**  
Project # 25059  
Date 05/14/2026

**Revisions**

**Sheet Title**  
**South Property Line Planting Plan**

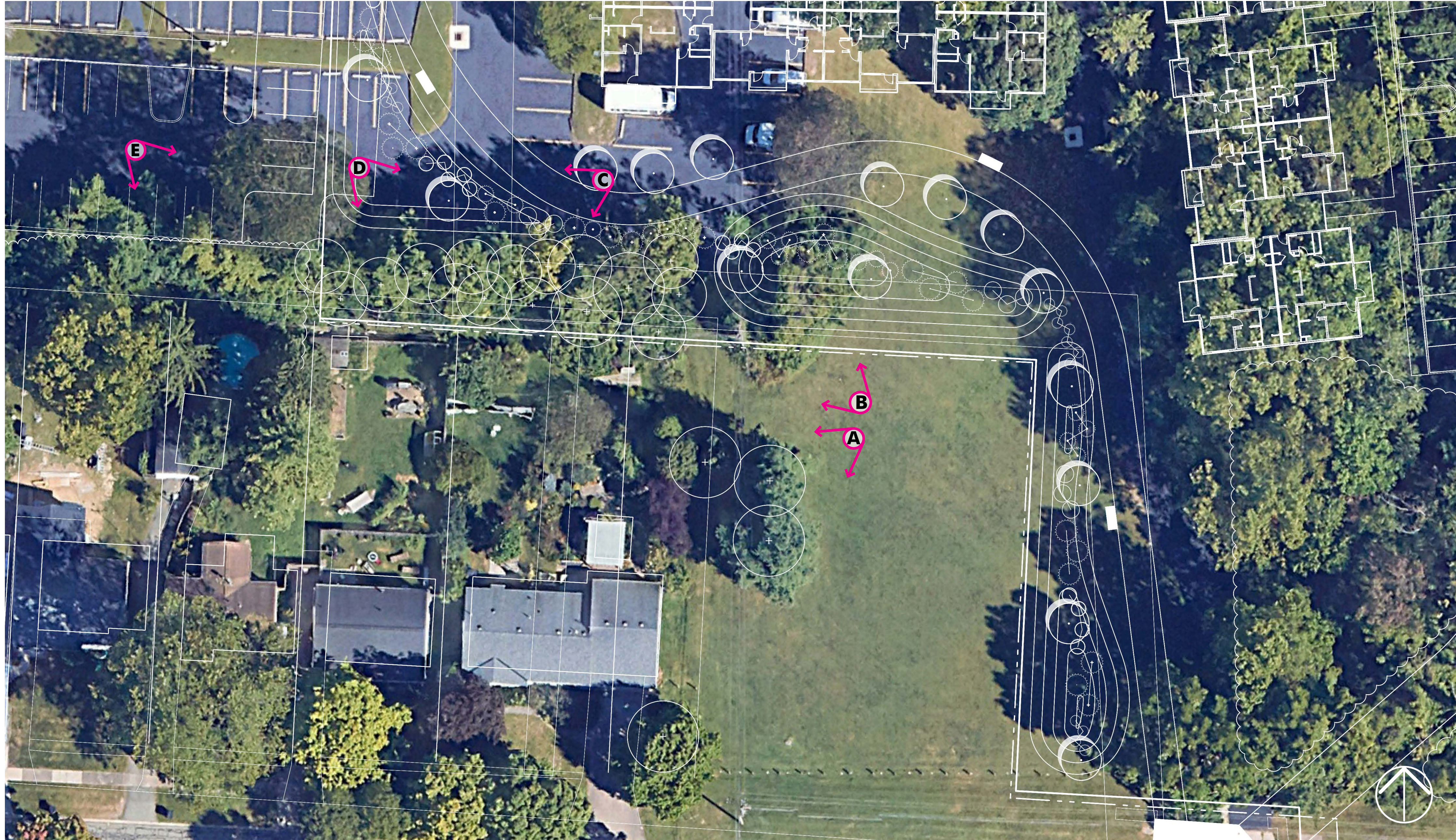


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**L2.1**



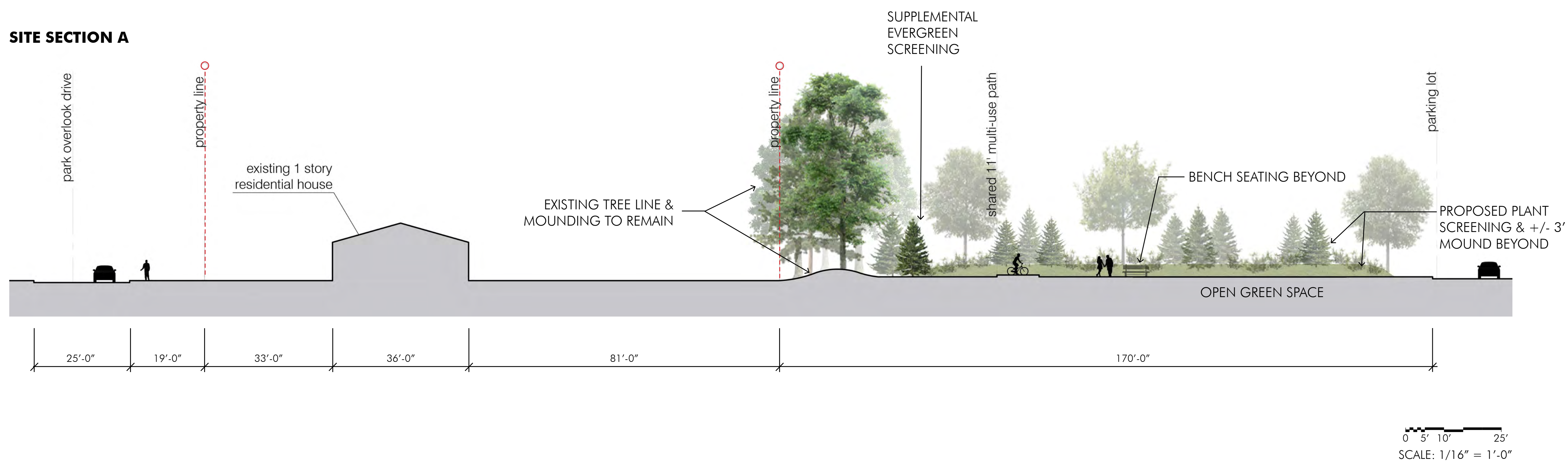
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**EXISTING CONDITIONS**

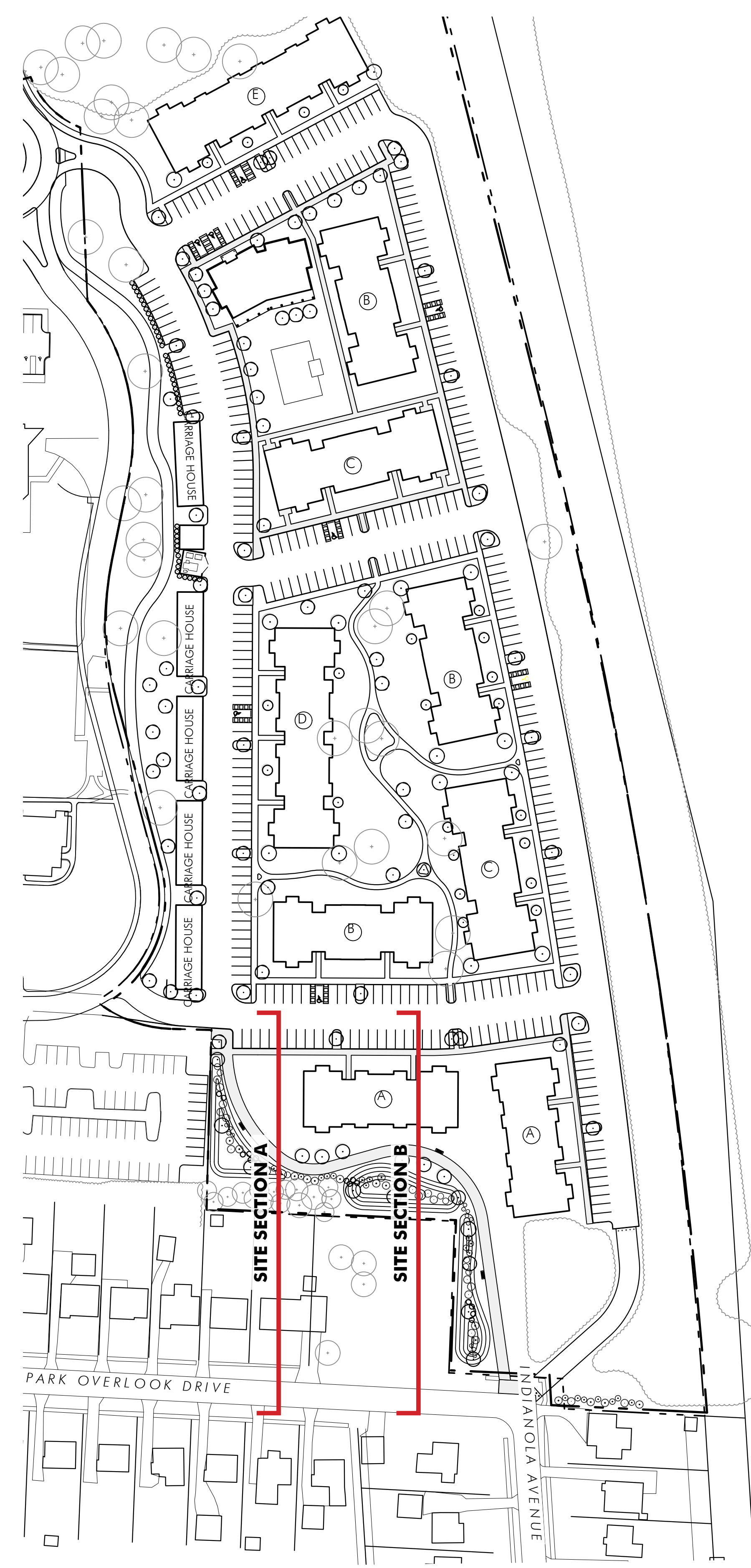
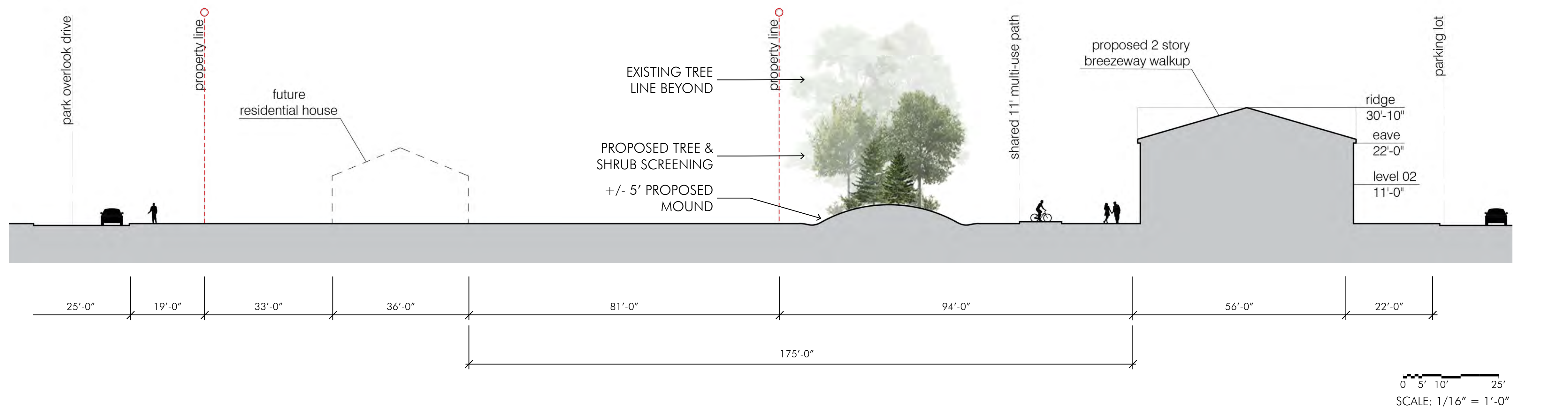


# Proposed Site Sections

## SITE SECTION A



## SITE SECTION B







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560 Park Overlook Dr

Existing Tree Line to Remain

Proposed 2 Story Walkup

Proposed Mounding and Landscaping Buffer

Proposed Multi-Use Path



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0 25 50 100'  
SCALE: 1" = 50'-0"



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# CONCEPTUAL SITE PLAN



BOUNDLESS CAMPUS

PROPOSED ROUNDABOUT

11' SHARED MULTI-USE PATH

PARKING BUFFER, TYP.

CARRIAGE HOUSE, TYP.

EXISTING RAVINE / WOODED AREAS TO BE PRESERVED, TYP.

MOUNDING & TREE / SHRUB SCREENING

BUILDING TYPE A

BUILDING TYPE B

BUILDING TYPE D

OPEN SPACE AMENITY

BUILDING TYPE B

BUILDING TYPE C

AMENITY AREA & POOL DECK

CLUBHOUSE

BUILDING TYPE B

BUILDING TYPE C

BUILDING TYPE E

INDIANOLA AVE.

PARK OVERLOOK DR.

11' SHARED MULTI-USE PATH

EMERGENCY ACCESS ONLY

CONTROLLED ACCESS (EX. BOLLARDS, GATE)



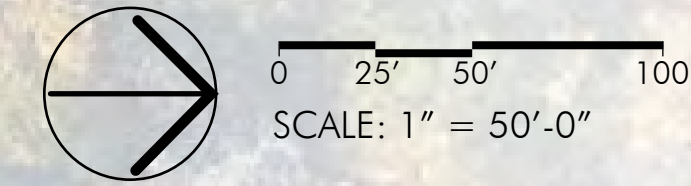
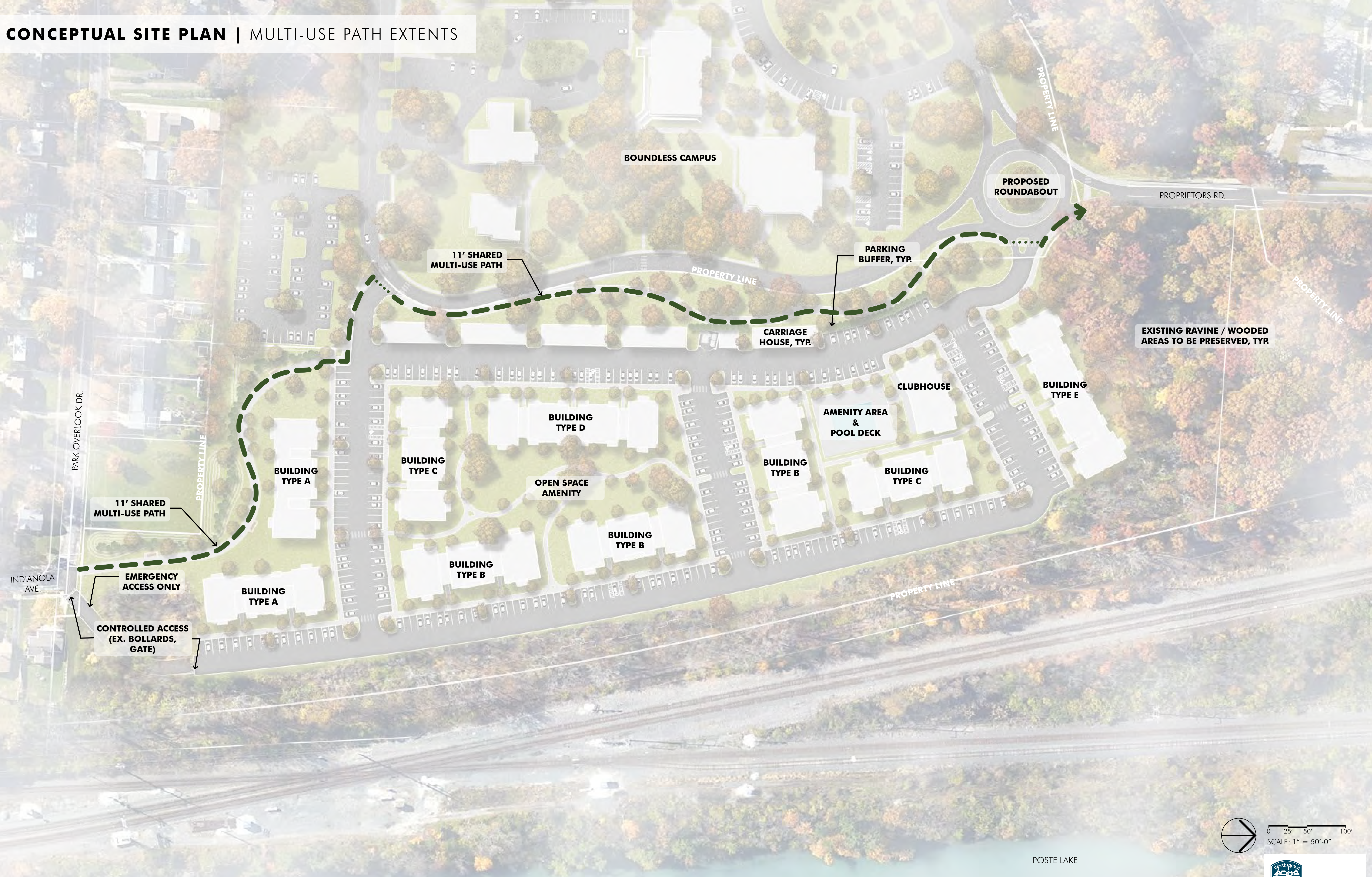
0 25 50 100'  
SCALE: 1" = 50'-0"

POSTE LAKE



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DATE 06/17/2026

**CONCEPTUAL SITE PLAN | MULTI-USE PATH EXTENTS**



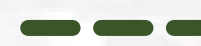

# CIRCULATION & OPEN SPACE

## LEGEND

### EXISTING PEDESTRIAN CIRCULATION

-  SIDEWALK
-  ROAD CROSSING

### PROPOSED PEDESTRIAN CIRCULATION (MULTI-FAMILY SITE)

-  MULTI-USE TRAIL (11' WIDE)
-  INTERNAL WALKWAYS
-  ROAD CROSSING

### PROPOSED PEDESTRIAN CIRCULATION (BOUNDLESS CAMPUS)

-  SIDEWALK
-  ROAD CROSSING



**BOUNDLESS CAMPUS**

**PROPOSED ROUNDABOUT**

PROPRIETORS RD.

**11' SHARED MULTI-USE PATH**

**EXISTING RAVINE / WOODED AREAS TO BE PRESERVED, TYR.**

**CARRIAGE HOUSE, TYR.**

**CLUBHOUSE**

**AMENITY AREA & POOL DECK**

**OPEN SPACE AMENITY (±1 ACRE)**

**11' SHARED MULTI-USE PATH**

**EMERGENCY ACCESS ONLY**

**CONTROLLED ACCESS (EX. BOLLARDS, GATE)**

INDIANOLA AVE.

PARK OVERLOOK DR.

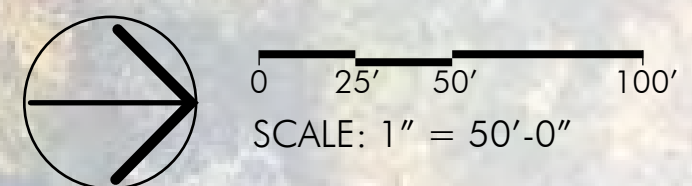
PROPERTY LINE

PROPERTY LINE

PROPERTY LINE

PROPERTY LINE

POSTE LAKE






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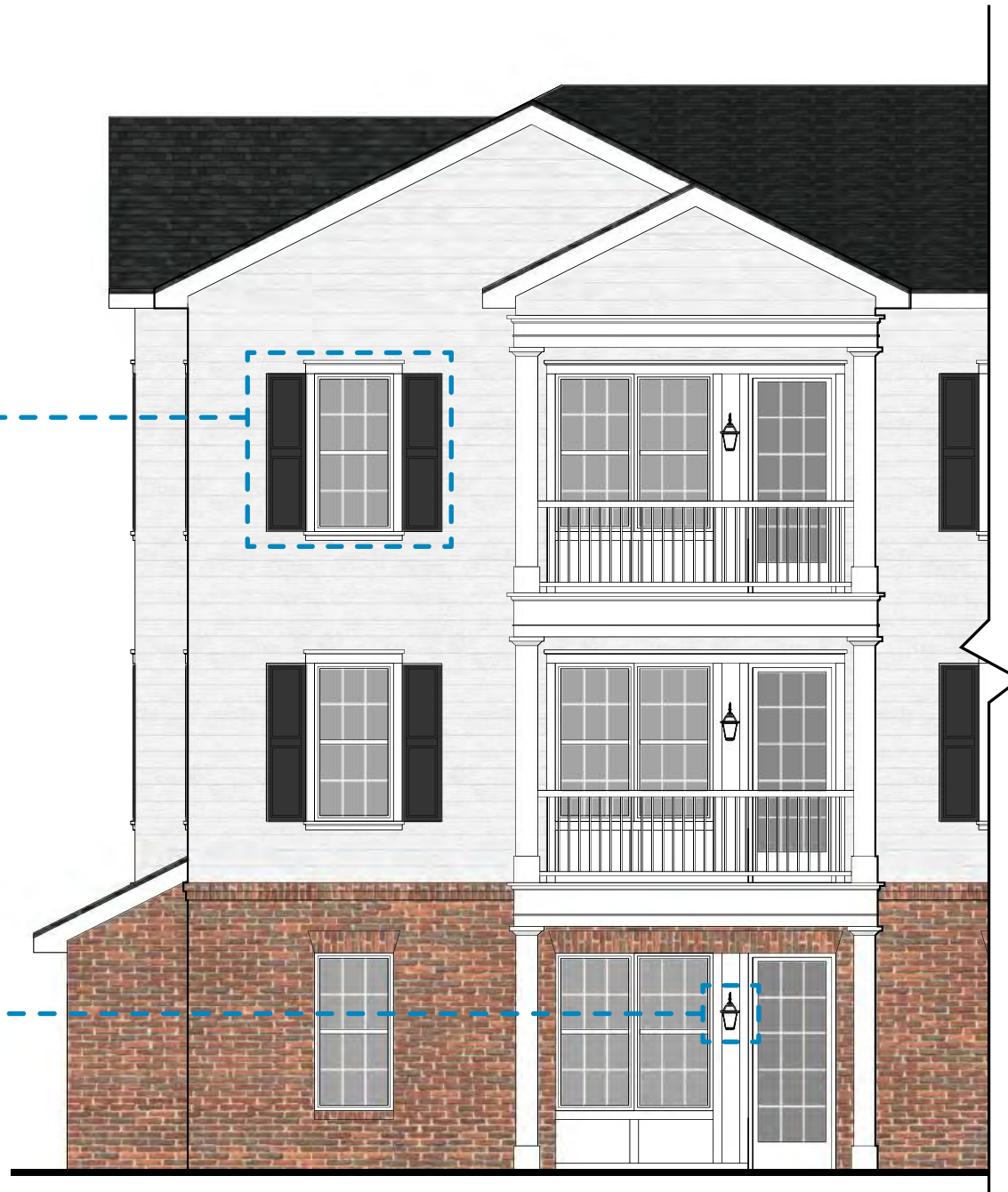
  
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Window Trim and Shutters



Traditional Sconce



Front Elevation - Building B  
N.T.S.



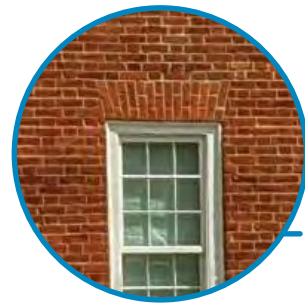
Front Elevation - Carriage House  
N.T.S.



Jack Arch



Carriage House Garage Doors



Jack Arch



Traditional Sconce



Front Elevation - Building A  
N.T.S.



Front Elevation - Building C  
N.T.S.



Balcony Entablature



Roof Bracket



Balcony Entablature



Jack Arch



Front Elevation - Building D  
N.T.S.



Front Elevation - Carriage House  
N.T.S.



Window Trim



Traditional Sconce

Open

Semi-Enclosed

Fully-Enclosed



Balcony Entablature



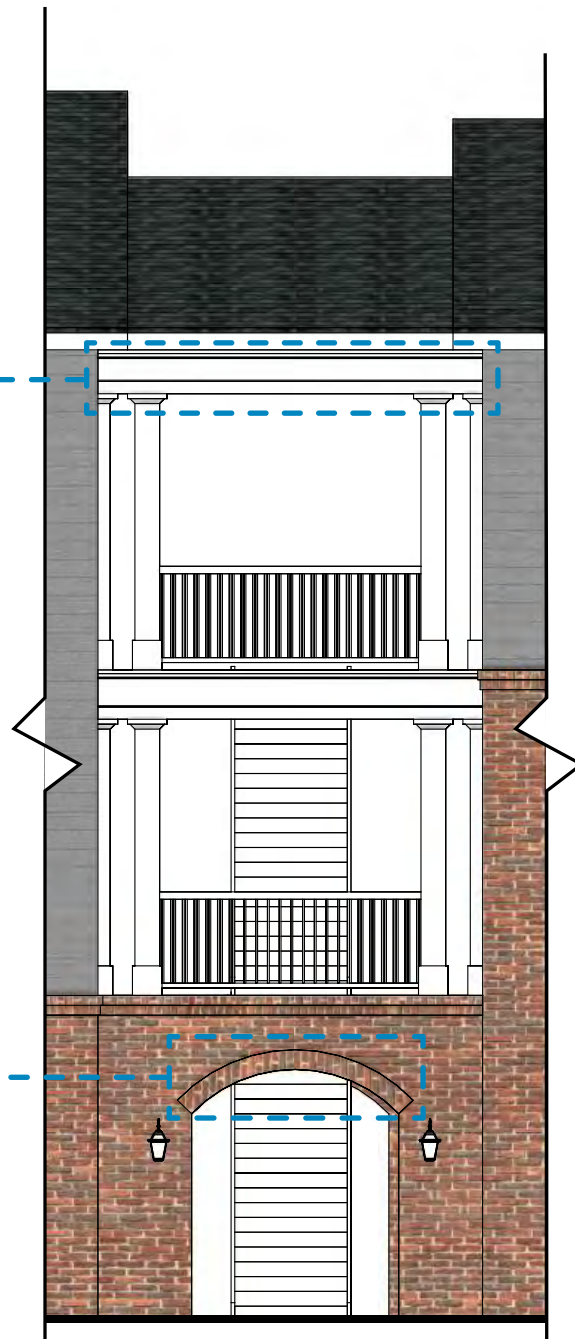
Segmented Arch



Window Trim



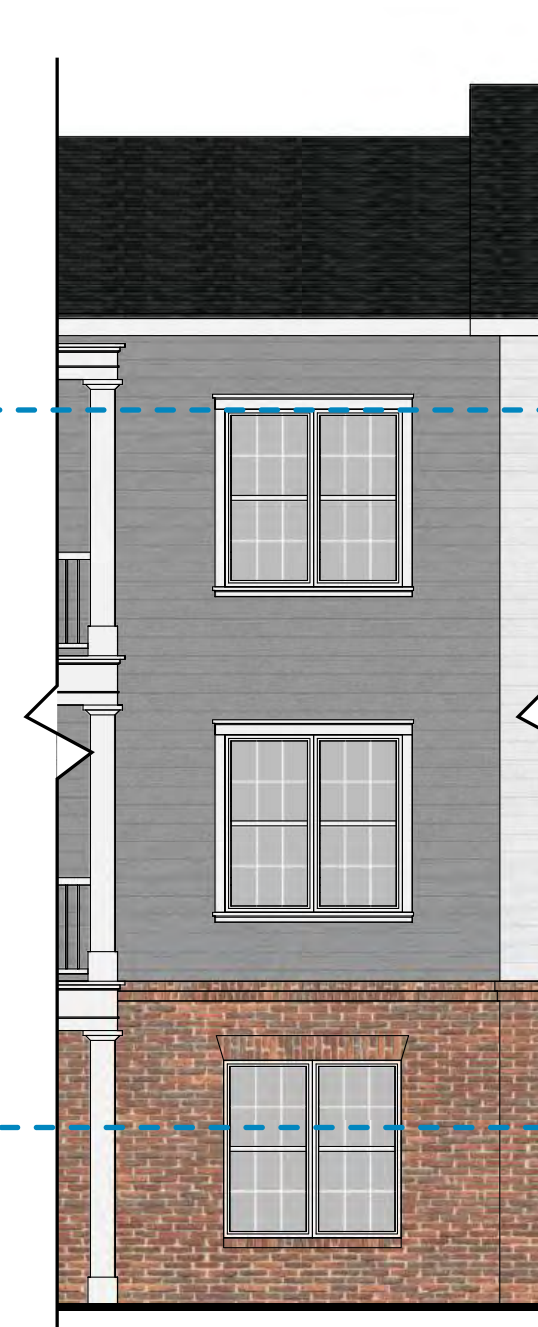
Traditional Sconce



Breezeway - Building C  
N.T.S.



Breezeway - Building D  
N.T.S.



Breezeway - Building E  
N.T.S.



Front Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"



**BR-01**  
material: brick  
manufacturer: continental brick company  
style: running bond  
color: 594 autumn blend  
grout: gray



**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white



**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm



**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white



**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



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Front Elevation  
1/8" = 1'-0"




Side Elevation  
1/8" = 1'-0"

- 


**BR-01**  
material: brick  
manufacturer: continental brick company  
style: running bond  
color: 594 autumn blend  
grout: gray
- 

**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white
- 

**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm
- 

**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white
- 

**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate

  
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Front Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"

**BR-01**  
material: brick  
manufacturer: continental brick company  
style: running bond  
color: 594 autumn blend  
grout: gray

**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white

**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm

**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white

**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



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DATE 06/17/2026



Front Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"



**BR-01**  
material: brick  
manufacturer: continental brick company  
style: running bond  
color: 594 autumn blend  
grout: gray



**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white



**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm



**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white



**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



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Front Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"



**BR-01**  
material: brick  
manufacturer: continental brick company  
style: running bond  
color: 594 autumn blend  
grout: gray



**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white



**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm



**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white



**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



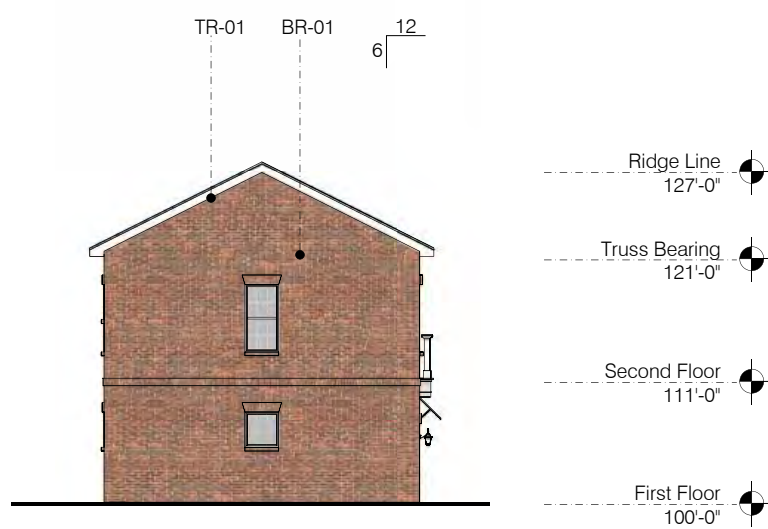
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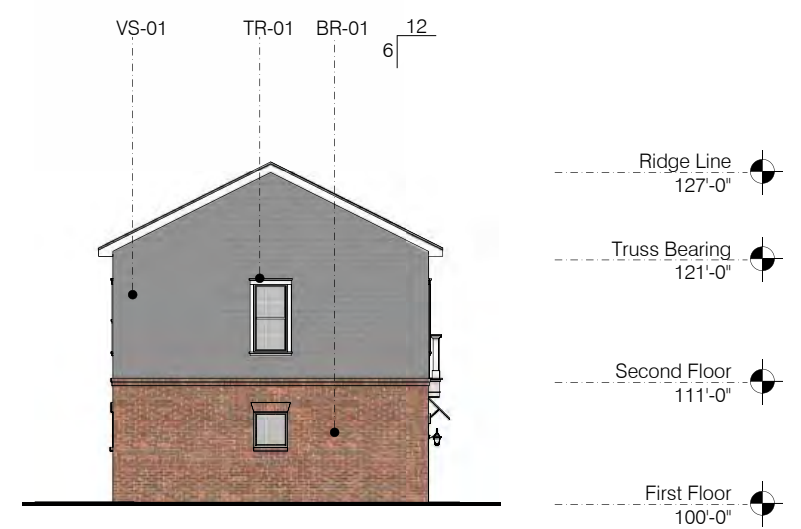
Front Elevation  
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Front Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"



Side Elevation  
1/8" = 1'-0"

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manufacturer: continental brick company  
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grout: gray

**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white

**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm

**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white

**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



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**BR-01**  
material: brick  
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style: running bond  
color: 594 autumn blend  
grout: gray



**VS-01**  
material: architectural grade composite siding  
manufacturer: tbd  
style: d4.5 clapboard  
color: glacier white



**VS-02**  
material: architectural grade composite siding  
manufacturer: james hardie (or eq.)  
style: d4.5 dutch lap  
color: storm



**TR-01**  
material: fiber cement panel  
manufacturer: james hardie (or eq.)  
style: trim board  
color: white



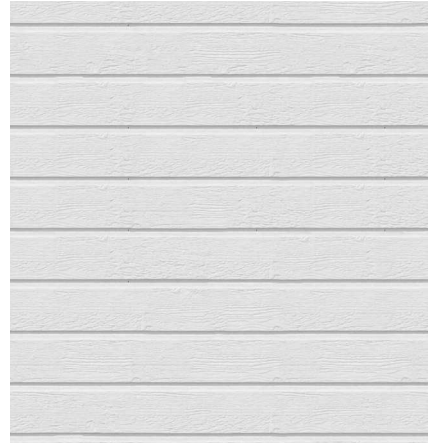
**AS-01**  
material: asphalt fiberglass dimensional shingles  
manufacturer: certainteed (or eq.)  
style: landmark  
color: colonial slate



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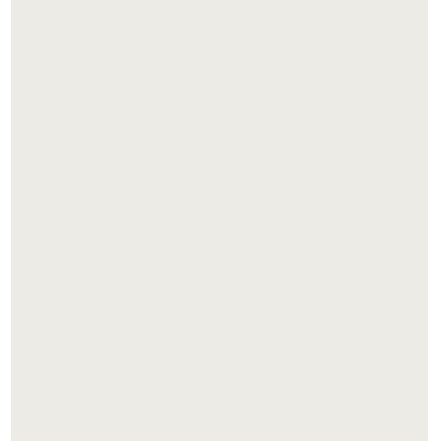
**BR-01**  
 material: brick  
 manufacturer: continental brick company  
 style: running bond  
 color: 594 autumn blend  
 grout: gray



**VS-01**  
 material: architectural grade composite siding  
 manufacturer: tbd  
 style: d4.5 clapboard  
 color: glacier white



**VS-02**  
 material: architectural grade composite siding  
 manufacturer: tbd  
 style: d4.5 dutch lap  
 color: storm



**TR-01**  
 material: fiber cement panel  
 manufacturer: tbd  
 style: trim board  
 color: white



**AS-01**  
 material: asphalt fiberglass dimensional shingles  
 manufacturer: certainteed (or eq.)  
 style: landmark  
 color: colonial slate